

1928

Kenya

No. 15287

SUBJECT

CO 533/379

1st Supplementary Estimates
1928

2nd, 3rd, and 4th

Previous

X 10450/27
(Of. B.O.)

Subsequent

(Spare copies to Library.)

I think not
1/25/28

- B. Payment to Nairobi ^{as to prominent} £7,932
- C. Nairobi Town Planning (Township) } 3000
- ✓ Nairobi Elementary Schools } £15,000

to I approve by tel. as requested.

£15,000 seems rather a lot of money for the state
 in educating the white children of Nairobi.
 It is £5,000 in excess of the preliminary
 estimate on p. 17 of the Rep^t of the Select
 Com^{tee} on S.A.S.

Head vin a. It wd I think be
 interesting to have the report which the
 C.V.C. undertake to lay on the table
 of council regarding famine.

H^o xxix Technical training
 Africans. (H. in Gov's Dep^t.) The

Departmental instructions have appeared
 not turned up. The recommendations
 of the Select Com^{tee} on this scheme
 are to be found on p. 31, 252 of their

See 15322/28

henceforward
See no. 8.

I think you are
 have been told
 I was at
 Disband
 1/25/28

* This is in
 business
 1/25/28

report (on Estimates paper)

We might follow up Mr. G
with asking for the C.S.G. report
in the frame & for the Dept.
instructions on African Technical
Training

Although, experimentally, this
scheme is to be under the P.W.D.
& of the Education Dept. Mr.
Visher, etc.

G. Eastwood
10/7

Appropriation Ordinance No. 4 of 1928

(1). These Estimates amount to £190,842.
Against this is anticipated savings of £111,052,
leaving a net excess of £79,790. In addition,
there are revotes amounting to £47,702. The
result of these votes, together with the revised
Estimate of Revenue for the year would appear
from the statement in 15276/28 to turn the
surplus of £10,191 originally estimated, into a
deficit of £261,486, but this sort of apparent
deficit is rather a common feature and there is
no reason to doubt the confident anticipation in
this despatch that there will actually be no
deficit at the end of the year. I see no reason
for withholding approval of any of the items
submitted, so telegraph in reply to this des-

despatch, simply saying "approved"
(This will cover both the Supplementary Estimates
and the revotes), but sending by despatch pointing
out as regards

(a) (page 2) that the confidential correspondence
should not have been referred to in a public despatch
but that he will however now have received the reply
sent to the confidential despatch in question.
(b) (page 4) refer to the Secretary of State's
reply on 15265/28.
(c) (page 5). I think it should be pointed out to
the Governor that the despatch to which he refers
dealt solely with the case of Mr. Webster and con-
tained no reference to this expenditure of £1,866.

(h) (page 6)
(i) (page 7). It may be pointed out that the
enclosures referred to in these sections were not
sent with the despatch, although copies in both
cases have been received in the Colonial Office
separately (see enclosure & (h) & (i) records)
J.M. Allen

2/7/28

As proposed both on (c) & (d)
then refer to the fact to
see para 4 F which may
require comment as regards

Allen
21.7.28

I think it will
be nice to see this
by J.C. letter to
information
Allen

This too can
go in the letter
to information
Allen

DESTROYED UNDER STATUTE

24 July 1928

2 AUG 1928

Gen 551 (1000)

DESTROYED UNDER STATUTE

Sw. 599 2 Aug 9. G/3

Colonial Secretary 31st July
Has 12 printed copies of the appropriate
Ordinances, No. 11 of 1928.
Spare copies to library.

J Seal
? Put
(Signature)
298

(This is marked to you to see)

Extract (para 4 F) of Vol. 65
be placed on the ~~file~~ papers
about Trypanosomiasis Research
at Entebbe; those pp. 15 be then
circulated to me

Done. R.H. 11/11
22/2501/9/28/29

J Seal
Do & at once.

Acting Colonial Secretary 11 Sept 1929
Now forwards enclosures omitted from vol.
his handwritten

These are the enclosures referred
to at * in minutes on page 1.
? put by

(Signature)

9. — 500 files 506. 12 Oct 1928
Has 12 copies of 2nd Supplementary Budget 1928-29
Comment: Request approved by telegram

(Spare copies to library)

Amount from the interest on the
£ 1/2 million loan: £ 200,000 only
amount of £ 36,250 - which is slightly
more than set off by new savings
estimated. The net excess is
now £ 7,325.

Some of the estimated savings
are noteworthy, e.g. Medical Dept:
£ 12,000 est., Education Dept £ 19,000 -
last year the savings on the
Education Dept were £ 20,000.
It seems undesirable that there
should be this large discrepancy between
estimated & actual expenditure, but
I suppose inevitable.
The method of which the £ 19,000

Some length in the study of
 & far as concerns the
 2nd Supplementary Sect. the
 writing to which exception
 need be taken & I have
 passed hel: approving
 the expenditure
 But I am not clear as
 to column 2 in the print
 & the position of items of
 expⁿ included in that
 column.

Recie. to hold as a
 this, for discussion, & please
 put up only pp. available
 as to the removal of
 of artistic materials, contribution
 of £5000 to the Corporation
 memorial & grant of
 £2500 to the East National
 History Museum.

to collecting to be
 with the removal of
 kind to hold (this
 is a proposal of which
 I am sure we shall
 be advised.

recd
 12.11.28
 at once

Para 2
 collect (see
 above)

to allow the... of the...
 to expire in the... 16
 apparently provision is not
 made in support... for expⁿ
 afforded by... in L.C.
 but the... is not
 according to... Recie.
 I am... to...
 the matter to...
 recd
 30.11.28

Reporte that...
 were...
 and...
 before the...
 items indicated.

Item 2. ~~£16526~~ £16526 was provided
 for the supply & transport services under
 Head xxix Public Works Dept. subhead
 Galt Transport. This was to give
 effect to the proposed re-organisation of the
 service. However in his telegram
 of 23rd Sept (No 3 on 15/11/28) the Secy
 stated that he had decided that the
 service must remain under the control of
 the O.C. troops as a disciplined body.
 Presumably therefore this is to give

effect to the change of mind, and
there will be a more or less corresponding
savings shown on the P.W.D. Head.
On the assumption we can
approve

Item 3. For road 3 of 1/2 mile on
13000 feet to adjoining 1/2 mile
to draw attention to the progress
in our letter of 12 Dec

Item 4. Court. This order was
approved on 5/12/26 by the
of which I attach a printed copy
(the paper not being available).

Item 5. Under the new Municipalities
Order the grant is to be for half
the upkeep of main trunk roads.

Item 6. An area of 60 acres
recently incorporated in the
township for the establishment
of a native location at ~~Silbert~~
(see p. 13 of the Batham Report vol. II)

not chief.
The Dept would not
withstand sanction to
be made. 1/11/26

Expenditure for this purpose is to be
to you & may be approved only
No comment is deemed necessary
concerning them

to be done by the
the rest of the
to be done by the
to be done by the

telegraph. Item 2. 3. Proposed
provision of 16,000
that corresponding
with Heat XXIX. Puffer looks Dept
Government transport. Item 3. cost
proposed in my letter of 12 Dec. Or
being completed expenditure approved

By Eastern
27/12/26

I agree generally with
two military bands

Quadrant
27/12/26

Ref. as in the Report of the
to be done as the Annual
Contribution
P. Allen

12, to say Tel. 28 Dec/26 (No 11 used)

State savings shown in respect of
Government Transport under Head 24
total £12,114. savings on other items
under Head 26 total £4,530. With
reference to Item 3. Insp. Gen. has approved?

No action required as to the
part of this letter. From the
Mr. Mackley for action on the
remainder, if required, & on the
two papers below (see Mr. Mackley's
summary of the ~~expenses~~
portion - his minute of
5/12/28 on 13080/12. All
action as regards taxation in the NFP
has been suspended pending discussion
with Sir S. Glegg here. It has previously
been considered sufficient to proceed with
the reduction of the Kead. by one
company, or proposal to elect the
Co. was prepared to agree - see p. 1 & 2
of his Inspector Report (11/1/28) on 13080
& his former letter to Mr. Mackley
of 30.11.28 on (no. 6 on 15313/28)

G. Jackson
2.1.29

Mr Parkin

minutes on 4 on 13080 28 $\frac{\text{min.}}{\text{Kead}}$
 $\frac{\text{Kead}}{\text{Kead}}$

no. 28. in the last inspection
report of the 3rd Lab (11 on 13080/28)
made general proposals for
reducing the wages. The
the result of the Lab. of 4 Dec
4 on 13080/28 which aimed for
wages suitable for the situation
of certain services & W.P. adjustment.
we had no objection, and in
part of clause was it by which
disposed to accept these general
proposals. In view of the fact
that the 4 on 13080 28
as to the expenditure for
which approval was sought
we made an approval subject
to the 26. committee on 26
being taken in clause. We finally
approved that the expenditure

note as to this.

It is clear that these payments to Major Miles should have been submitted to the Secretary of State for prior approval; but I presume that ~~the~~ covering sanction must be given in the case of the other exceptional payments in respect of passage and outfit allowance which are referred to in paras. 2 and 3 of the Secretary of State's despatch of the 26th January 1927 on P375/26.

Subject to Mr. Machtig's observations as regards military expenditure, I approve expenditure in these Estimates in so far as they have not already been approved in the S. of S.'s telegram of the 26th December (No. 12). Say that although the Secretary of State does not withhold approval of the payment of salary to Major Miles from Kenya funds, (presumably at the rate of £400 a year for the months of July to October 1926, these payments should have been submitted for approval since, apart from the fact that the salary of the post of Aide-de-Camp, for which provision ^{is} made in the Estimates, was being drawn by another officer, the S. of S.'s concurrence in the employment of Major Miles as Senior Aide-de-Camp was given on the understanding that it would not involve any extra expense from public funds on General's issue staff. Point out that in the S. of S.'s despatch No. 170 of the 26th of January 1927 (referred to above) dealt with certain other instances in which his sanction for unauthorised payments in respect of members of the Governor's personal staff had only been sought after provision had been made in the supplementary Estimates and approved by the

Legislative

Legislative Council; and express the hope that in future the Secretary of State's approval will be sought before any such payments are made and before the Legislative Council is invited to make provision for such purposes.

J.H. Miller
27/1/27

In the two military items (viz. 13 & 14 items & (10/11)) we made it clear to say by the fact that 13's concurrence in these particular purchases will be obtained (see 12 and 14 below). No 1 in 1889/29 since received indicates that the R. Co. that further vehicles will be necessary but it may be assumed from it that he is in agreement with the purchase and is present.

In approving these items we might refer to the earlier R.C. Committee's finding in No. 1 in

15559/29
15559/29
15559/29

has expenses to be incurred
material purchased

15559/29
material purchased

E. H. ...

The ... is ...

...

All P

27.2.29

To Sec. Conf. 15 April

...

...

...

...

...

30/3/29

...

I have ... arranged ...
to call ...
the paper ...
when ...
ask ...

Mr. Martin called to-day, and I asked him
about the point referred to in Mr. Parkinson's minute
of the 27th of November 1928. He explained that the
second column is inserted for the convenience of
members of the Council in order that they may know
which of the expenditure represents amounts already
approved separately, and how much is really new
expenditure. The Council therefore only approved
in the Supplementary Estimates the amounts shown
in the first column under "amounts now asked for";
these in the second column having already been
approved; but the amounts in both columns require
of course, the approval of the Secretary of State
and that is given by general approval of the
same in the second column form part of the
Supplementary Estimates.

But by for Mr. Parkinson to see on his
return.

for
837926

...

9/4

Thank ...
All P

On another paper we have sent a
despatch to Kenya, expressing appreciation of
the additional assistance which they gave to

Amount during 1928, so no action is required on this paper.

G.A.C. 312, 22 JUNE 1929
Transmit for approval the Fourth Supplementary Estimates, 1928 with explanatory memorandum.

Two points:

Head (xxv) Salt Coast Agency. Enclosure to the financial statement of 1928 in excess under this head is very one. The adjustment is 1/3 of certain specific means in the former tax following agreement with regard to certain matters. The fact that money is (10000/2) the 0.004 reported in enclosure of the agreement & promised later to submit particulars of the 1928's approval. No trace of such submission can

be found
in 1912 & 1928/29 but then
apparently a full amount
was not paid. 16.16/18

has been obtained

Enclosure to Royal Warrant No. 9
of 1928. Page 5 of No. 9
for the year 1928. The total
to the account number 100,000
in February. Subsequent

1928. Subsequent
1928. Subsequent
1928. Subsequent

To Gen. 663. 17 August 22 AUG 1928

~~Mr. E. ...~~
any action necessary...

There is up in report of ...
in 1928. Salt (imp. 17) ...
(cost of money; but ...)
of 1928. enclosing up from
2 of (18) when particulars
of the agreement may be
referred

11/22/28 4000 13

Mr. [unclear]

- Mr. [unclear]
- Mr. [unclear]
- Mr. E. Harding
- Sir J. Shackleton
- Sir G. Grindle
- Sir C. Davis
- Sir S. Wilson
- Mr. Ormsby Gore
- Lord Lovat
- Mr. [unclear]

10.0
10.0
10.0

22 Aug 28

DRAFT 4000 No. 663

Gov. 5/29/99

I have the honor to acknowledge the receipt of the letter of the 22nd June in which he forwarded the draft Supplementary Estimates for the year 1928, and to convey to you my appreciation for the expedition of the enclosed draft Estimates.

(2) As regards
 XXVII Govt. Expenditure Agency, 1/2/28

46/16
Tele.gram from the Officer Administering the Government of Kenya
to the Secretary of State for the Colonies.

Dated 22nd December, 1928.

(Received Colonial Office 4.50 p.m. 22nd December, 1928.)

Confidential. No. 345. 22nd December. Three supplementary
estimates passed in the Legislative Council yesterday include
the following items in respect of which I should be glad of your
decision by telegram if possible before the end of the year.

(1) Pensions and gratuities £10,000 to cover cost of
gratuities payable during 1928 under European Officers Pension
Ordinance. (2) Sugar and Transport Services King's African
Rifles £10,000 see paragraph 5 my telegram of 25th August
Confidential No. 344 and memorandum on estimates for 1929
page 28. (3) Wireless installation(s) for northern frontier
posts and Turina £286 and 6 lorries and equipment for
first line military transport in those areas £3,500 see your
telegram of 12th December. (4) Expenses in connection with forest
destruction £10,525 to cover cost during 1928 of large
organization now engaged in forest destruction. (5) Acquisition of
land for Eldoret Schools £2,467 for acquisition of certain land
in Kapobaya township adjacent to Eldoret required for extension of
school grounds. (6) Contribution to Nairobi Municipality towards
 upkeep of trunk roads £0,000 representing Government liability for
one half of cost of construction and maintenance of 3 trunk
roads and a proportionate estimate of sum allowed for
purchase of machinery. (7) Eldoret Native Location £3700
representing Government half share of cost of establishing a
native location on proper lines in Eldoret. (8) Loss and depreci-
ation of stores £3500 stock in Public Works Department.
Unallocated Store has recently been verified after lapse of
nearly 6 years and it is desired to write off accumulated shortage
chiefly due to breakages and errors in nomenclature amounting to

15713/25
1559/25
15713/25
1559/25
15713/25
1559/25

6
\$5,500. Full report will be sent by next mail.

My telegram of 11th December No. 534. Additional provision of \$500 has been made for contribution to fund.

Despatches follow by mail forwarding third supplementary estimates.



KENYA

GOVERNMENT HOUSE,
NAIROBI,
KENYA

No 566

October, 1928

Sir,

not

2nd. Supplementary Estimates.

Report of Select Committee.

With reference to Kenya Despatch No. 297 of the 2nd June, 1928, I have the honour to transmit for your consideration twelve copies of the Second Supplementary Estimates for 1928, as passed by the Legislative Council on the 30th August, 1928, together with two copies of the report of the Select Committee of Legislative Council appointed to consider them.

2. It will be observed that the net excess of additional sums now voted over expected savings is £79,321. There is, however, no doubt that this sum will be fully covered by the surplus balances which may be expected in respect of the current year.

3. I would also invite attention to the fact that of the total sum of £88,242 now voted no less than £52,500 is for the payment of interest on the loan of £3,500,000 raised under the provisions of Ordinance No. 22 of 1927.

THE RIGHT HONOURABLE
LIEUTENANT-COLONEL L. G. M. S. AMERY, P.C., M.P.,
SECRETARY OF STATE FOR THE COLONIES,
DOWNING STREET,
LONDON, S.W.

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-2-

4. The items included in these Supplementary Estimates are generally dealt with in the printed explanatory memorandum, but I would add the following observations in addition to those explanations:-

A. MEDICAL DEPARTMENT, EXTRAORDINARY.
Special Anti-Malarial Measures.

The outbreaks of malaria which have occurred in recent years, and in particular the appearance of anopheles mosquitoes in places where they had not previously been known to exist in any appreciable quantity, have been causing my medical advisers considerable anxiety. The combating of this disease has been commenced in Nairobi, as already reported to you in connection with the main Estimates for 1928, and a sum of £1,500 has been included in the Supplementary Estimates now submitted to provide for an entomological survey of certain areas, in particular the Trans Nzoia, where malaria has been lately making an appearance, and also for the purchase of additional quantities of quinine.

B. AGRICULTURAL DEPARTMENT.
Expenses of Marine Biologist. £750.

It is proposed to commence the survey of the fisheries at the Coast this year and the sum provided in the Estimates for 1928 will not suffice for the expenses which are likely to be incurred.

As you are aware, the Governments of Zanzibar and Tanganyika Territory have agreed to share in the cost of this sum.

C. AGRICULTURAL DEPARTMENT - EXTRAORDINARY.

The sum of £1,250 inserted for machinery for the Cool Stores is provided to enable the installation of duplicate machinery and plant in order to obviate the grave risk of considerable loss in the event of a breakdown in the present machinery.

D. EXPENSES IN CONNECTION WITH LOCUSTS - £6,500.

The costs in connection with this service have now been ascertained and in consequence the sum of £3,000 originally provided has been increased. In this connection I would mention the services rendered by the Government of Portuguese East Africa and their expert, Dr. Fuller. This officer visited the country and was of the greatest assistance in dealing with the locust menace which threatened to become very serious. The Government of Portuguese East Africa generously paid his salary and travelling expenses to the Colony.

E. MISCELLANEOUS SERVICES.

Grant to assist in the education of the son of the late Sir Christian Felling - £2,000.

The son of the late Sir Christian Felling has only very recently entered the University of Oxford.

Oxford, and it was the wish of his father that he should take his degree there and also be called to the bar. It has been ascertained that his father's estate will not permit of this wish being carried out and while it is felt that any necessary provision for the other members of Sir Christian's family may be left for the consideration of the Railway Advisory Council, it is considered that this Colony should take the opportunity of recording its appreciation of the high value that it places on the services of the deceased officer not only to the railway, but to the Colony, by providing for the future of his son. The sum inserted has been calculated on the probable cost of the remaining period to be spent at Oxford and for two years to be spent reading in Chambers for the Bar. I would add that this provision was inserted with the hearty and unanimous support of the Elected Members of the Legislative Council.

1. EXPENSES OF ROYAL VISIT. £9,000.

In the Supplementary Estimates as originally drafted a total sum of £1,000 was provided for this purpose, but it is the case that liabilities amounting to some £8,000 have already been incurred. It was therefore intended to increase this sum to £9,000, but owing to an error in the drafting of the report

of the Select Committee, the sum actually voted by the Legislative Council was £10,000 in all. The error is, however, not material as the total expenses will undoubtedly exceed the sum now provided.

C. PUBLIC WORKS EXTRAORDINARY
Approaches to the ferry Kilifi Creek. £800.

These approaches are in a dangerous condition, one of them is very steep and narrow, and a fatal accident has recently taken place there.

I trust that these Estimates will meet with your approval and I shall be grateful if that approval may be intimated to me by telegram.

I have the honour to be,

Sir,

Your most obedient, humble servant,

Edward Gigg

G. O V E R N O R.

SELECT COMMITTEE'S REPORT
SECOND SUPPLEMENTARY ESTIMATES, 1928.

Your Excellency,

The Select Committee appointed by you to consider the Second Supplementary Estimates 1928 has the honour to submit its Report as hereunder.

2. The Committee notes that it is proposed to move the headquarters of the Northern Frontier Province to Isiolo, that the first steps required will be the construction of a bridge across the Uaso Nyiro and the completion of a house, already partly erected, for the Senior Commissioner, and the renewal of the telegraph line. The Committee recommends that provision for these be included in the next Supplementary Estimates and that the telegraph line be constructed to follow, if possible, the railway and the main road. The Committee further notes that provision for the removal of the K.S.R. etc., to Isiolo will be submitted by Government to the Council in due course.

3. Captain Kenealy asks that further consideration may be given by Government to the establishment of a Police Post at Marro Moru, this being now railhead.

4. Mr. O'Shea noted that the Senior Commissioner, Eldoret will be instructed to report on the Government buildings required there and the alienation plots on the main street.

5. The Committee recommends that enquiries be instituted as to the possibility of charging a low rate of interest to Government officers who receive advances from Government in order to buy motor cars and suggests in that

connection that, in such case, it might be advisable for Government to advance the whole cost of the car and not 90% as at present.

6. The Com^{tee} recommends the inclusion in the Supplementary Estimates of a sum of £800 for the improvement of the approaches to the Ferry at Kiliz Creek and further recommends that a scheme for an efficient ferry at Likoni shall be investigated without delay.

7. The Committee recommends that the sum of £2,000 be inserted in the Supplementary Estimates to provide for the education at Oxford in the Inns of Court and in Chambers of the sum of the 1945-46 C. C. C. Ann. Felling.

8. The inclusion in the Supplementary Estimates of a sum of £9,000 towards the expenses as so far ascertained in connection with the entertainment of His Royal Highness the Prince of Wales is also recommended.

9. After hearing the views of the Director of Agriculture the Committee recommends that the sum of £5,900 included in the Supplementary Estimates for Locust Destruction be increased to £6,500.

10. The Committee further recommends, after hearing the views of the Director of Agriculture, that the following additional provisions be inserted:

- (a) £100 for a boiler for the Maize Conditioning Plant.
- (b) £1,250 for machinery for the Cool Stores.
- (c) £105 to provide for the engagement of an additional temporary grader for three months.

11. On the advice of the Director of Medical and Sanitary Services the Committee recommends that a sum of £1,100 be provided for an Anti-Malarial Survey in the Trans Nzoia and a sum of £400 for purchase of quinine.

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The Committee notes the unsatisfactory state of the Laboratory at Nairobi and strongly recommends that provision for a new and adequate laboratory near the Native Hospital be made in the next Supplementary Estimates and that the work of construction be commenced as soon as possible. In the meantime the getting out of plans and final estimates should be proceeded with forthwith.

12. The Committee recommends the insertion of a sum of £750 to provide for a survey of the Fisheries of the Coast and notes that the expert from South Africa is expected to commence this work during the present year.

13. The Committee finally desires to record its opinion that Returns of Excesses on Subheads met from savings on other subheads of the same Head can be best dealt with by reference first to a Select Committee on Finance prior to their being laid on the table of the Council.

14. A schedule showing the alterations which will be necessary to the Second Supplementary Estimates, if this Report is adopted, is attached. These amendments will raise the sum asked for in these Estimates to a total of £88,212.

I have the honour to be,

Sir,

Your Excellency's obedient servant

Sd/- H. F. MARTIN

CHAIRMAN (FOR THE COMMITTEE).

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SCHEDULE

(1)	HEAD VIII	(a) Medical Department Extraordinary	
	<u>ADD</u>	Special Anti-Malarial Measures	£1,500
(2)	HEAD XXI	Agricultural Department	
	<u>ADD</u>	Administrative and General, Other Charges Item 35 Expenses of Marine Biologist	£ 750
		Personal Emoluments, Train Conditioning & Cool Stores Services	£ 195
(3)	HEAD XXII	(a) Agricultural Department Extraordinary	
	<u>ADD</u>	(1) New Boiler for Maize Conditioning Plant.	400
	<u>ALTER</u>	(2) Machinery for Cool Stores the item	£1,250
		Expenses in connection with Locust Destruction from £3,000 to £6,500 i.e.	
		add	£3,500
(4)	HEAD X.VII	- MISCELLANEOUS SERVICES.	
	<u>ADD</u>	(1) Grant to assist in the education of the son of the late Sir Christian Felling	£2,000
		(2) Entertainment of H.R.H. the Prince of Wales.	£3,000
(5)	HEAD XXI	- Public Works Extraordinary	
	<u>ADD</u>	Approaches to the Ferry Killifi Creek	£1,800
		Total	£19,705

Total of Supplementary Estimates
as printed in column 4 of page 4 68,937
£88,242

With the consequential amendments that in column 5 the figures £332,286 be substituted for the figures £312,981 and that in column 6 the figures £79,321 be substituted for the figures £60,016.

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COLONY AND PROTECTORATE OF KENYA

SECOND SUPPLEMENTARY ESTIMATES

1928

Second Supplementary Estimates, 1928.

HEAD AND SUBHEAD	Stationed Estimates 1928	Amount Approved by Motions in 1927 Council	Amount of 1st Suppl. Estimates 1928	Amount now asked for	Total Suppl. Estimates (voted and to be voted)	Expected Savings
I. PUBLIC DEBT FUNDED						
Interest—Ordinance No. 22 of 1927— Loan of £3,000,000 at 5%			125,000		125,000	
Interest—Ordinance No. 22 of 1927— Loan of £1,500,000 at 4%				52,500	52,500	
			125,000	52,500	177,500	
IV. THE GOVERNOR						
Personal Emoluments	11,638		100		100	
IVa. THE GOVERNOR EXTRAORDINARY						
Purchase of Motor Vehicles			875		875	
VII. PRINTING AND STATIONERY						
	37,727					187
VIII. ADMINISTRATION						
	271,678					3,000
VIIIa. ADMINISTRATION EXTRAORDINARY						
Mombasa Fire Brigade		61			61	
Motor-Lorry, Kamburi	250		115,500		110	
Fabrine Relief, Haringo District					500	
	250	61	115,500		671	
IX. STATISTICS AND RESEARCH						
	21,661					495
X. TREASURY						
Personal Emoluments	27,587		209		209	
XV. LEGAL DEPARTMENT						
	10,340					479
XVI. POLICE						
	143,864					7,000
XVII. PRISONS						
Personal Emoluments	20,428		105		105	
XVIII. MEDICAL DEPARTMENT						
	204,801					12,360
XVIIIa. MEDICAL DEPARTMENT EXTRAORDINARY						
Motor Ambulance				520	520	
				250	250	
XIX. EDUCATION DEPARTMENT						
	158,241					10,000
XIXa. EDUCATION DEPARTMENT EXTRAORDINARY						
Native Industrial Training Depot Equipment		103			103	
XXI. POST OFFICE AND TELEGRAPHS						
	151,199					538
XXIa. POST OFFICE AND TELEGRAPHS EXTRAORDINARY						
Replacement of Nairobi Telephone Exchange by an Underground System			5		15,750	
Aerial Telegraph Cable			114		50	
			2,500		9,550	
<i>Carried forward</i>		9,851	27,127	13,750	169,810	34,954
				57,157	177,860	



THE SECRETARIAT
NAIROBI
KENYA

62
8

AS

WHEN REPLYING
PLEASE QUOTE
NO. 5
Supp. 28/1/23.

September, 1928.

RECEIVED
46007 1928
COL. OFFICE

The Acting Colonial Secretary of the Colony and Protectorate of Kenya presents his compliments to the Under Secretary of State for the Colonies and has the honour to transmit copies of the Departmental Instructions issued by the Director of Public Works and of Secretariat Circular No.1 of 1928, which was inadvertently omitted from among the enclosures of Kenya despatch No.297 of the 2nd June, 1928.

No. 1

COMMERCIAL TRAINING OF ARTISANS

All concerned are doubtless aware of the schemes which are now being employed by this Department to further the training of artisans, and the arrangements made to meet the cost of the scheme fully and sensibly to allow

NATIVE INDUSTRIAL TRAINING UNDER ARTISANS' EDUCATION DEPARTMENT

During the period in which gangs are employed, cost is met in the proper manner for the work on which they are employed, and they are paid with all costs with the exception of certain items. The gangs consist of the artificers, viz. European fiddlers, artisans, and are composed of masons, carpenters, plumbers and painters. The situation does not have a satisfactory result, and very few of the gangs will be continually employed. This is because and the result of the failure of the leading artisans to carry out their work from the work on which they are employed, and are not employed by this Department, there will be a big loss to the Government.

PRISON SCHEMES

At present there are employed 5 instructors whose salaries are charged to the Department, and to the works on which the prisoners are engaged. The work of the prisoners carried out under the supervision of the instructors has been extremely good, both from the point of view of quality, quantity and economy. In the case of the hand works at Kital and Elguta special prisoners have been used, and the cost of these works, carried out the works on which the prisoners have been employed.

PUBLIC WORKS DEPARTMENT, SCHEME OF TRAINING

This scheme is divided into

into two heads of training --

- (a) Africans working in contact with skilled Indians.
- (b) By means of the gang system.

Arrangements are now being made to recruit supervisors, and it is hoped that every effort will be made to ensure that the scheme will be successful. The essence of both the above methods of training is that European Supervisors will be in continual attendance to correct mistakes and for instructional purposes.

It will thus be realised on perusal of the short explanation given above that in order to make all the schemes satisfactory from an economical and practical point of view the whole hearted co-operation of each Divisional Officer and his staff must be obtained.

Divisional Officers -- not only consider their immediate requirements but must arrange a programme of work in advance.

Monthly report should be forwarded monthly giving the following information --

- (a) Number of African artisans or Apprentices employed in the Division and name of work on which employed.
- (b) Nature of work on which employed.
- (c) Approximate value of work done.
- (d) Short account of work done.

On completion of each work a report should be forwarded to Head Office in which the information should be tabulated --

- (i) Date of Commencement.
- (ii) Date of Completion.
- (iii) Total number of Apprentices or African boys employed.
- (iv) Estimated cost of work.
- (v) Completed cost of work.
- (vi) Any useful information with regard to actual construction.

COLONY AND PROTECTORATE OF KENYA

CIRCULAR No. 1

GOVERNMENT SECRETARIAT

N. 1108

7th January 1928

CORRESPONDENCE WITH GOVERNMENT

Reference Paragraph 6 - Code of Regulations

The accompanying changes in the allocation of work in the Secretariat as well as between that office, the Land and the Native Affairs Departments are notified for the guidance of Heads of Departments and will take effect as from the 9th January 1928.

In order to expedite disposal of correspondence Heads of Departments are asked to ensure that communications are submitted in separate envelopes in so far as the subject matter concerns different sections of the new organisation.

The section prefix, e.g., "A" or "B," etc., as noted in the accompanying schedule, should be marked in the top left-hand corner of the envelope.

2. As the Native Affairs Department and Land Department have been incorporated as part of the Headquarter organization of Government, the Chief Native Commissioner as well as the Commissioner of Local Government, Lands and Settlement, have been authorised by His Excellency the Governor to communicate decisions of Government direct to the Heads of Departments concerned on matters primarily affecting their Departments.

3. No change is indicated by these arrangements in the instructions governing the channel of communication for the correspondence of Senior Commissioners or District Officers.

4. Opportunity is taken to invite special attention to the standing instructions embodied in paragraph 206 of the Code of Regulations. The main principles may be summarised as under:

(a) Matters in which other Departments are concerned should be taken up in the first instance with those Departments, and should be referred to Headquarters only if such covering authority is required or a settlement cannot be reached after such consultation.

In this event, the opinion should be clearly and fully stated with the comments of all Departments affected. To save time, there is of course, no objection in cases where only general sanction is required, to the submission of proposals through the Head of another Department, viz., the Hon. Treasurer on questions concerning finance, or the Hon. the Attorney General in regard to a legal matter. Meetings should be arranged between Heads of Departments for the discussion of subjects affecting their Departments before the submission of joint proposals to Government.

10. Officials of subordinate departments, in their letters forwarding the communications of their subordinates as objects to Government, state their own opinions and recommendations. They are required to do the same when reporting on any communications made direct to Government and referred to them.

11. Communications should be in memorandum form on foolscap size paper and should be divided into numbered paragraphs indicating actual and abstract references and date of receipt. Unless strictly condensed the memorandum should be typewritten. One side of the paper only is to be written on, the margin of at least a quarter of the width of the paper should be kept on the left-hand side.

12. Each memorandum should be confined to a single subject, which should be summarised in an appropriate heading.

13. Care should be taken to give references to last connected papers at the head of the memorandum.

14. Types of enclosures, wherever practicable, should be copied as far as in general cannot be obtained. Paragraphs of enclosures must also be numbered. It is desirable to avoid submitting a number of enclosures when this is the liable a number of the enclosures should be made in the covering memorandum.

15. Whenever information or advice can be obtained by telephone communication this means should be used and correspondence saved. In the case of things it may be necessary to get them recorded in writing but even this can always be made by telephone in the first instance with a view to obtaining presents or obtainable further information. It is arranged in all cases where correspondence can be saved.

16. Sections 14 and 15, No. 30, dated the 10th April, 1923; No. 245, dated the 1st May, 1924; and No. 15, dated the 1st March, 1926, are hereby cancelled.

EDWARD DENHAM,
Colonial Secretary.

To—
All Heads of Departments.
Schen, District and Resident Commissioners.
General Manager, Kenya and Uganda Railway.
Secretary, Governors Conference.
Secretary to the High Commissioner for Transport.
His Majesty's Trade Commissioner.
Private Secretary, Government House.

Sonnyre

Education.
Game and Fisheries.
Immigration and Emigration.
Juganese, Italian and Abyssinian Affairs.
Judicial.
Legislation—General and Unspecified.
Military and Defence.
Police.
Prisons.

B

Audit.
Asiatics.
Broadcasting and Wireless.
Customs.
Film Censorship.
Geology.
Legals.
Miscellaneous and Unallocated.
Posts and Telegraphs.
Public Works—other than Roads and Water.
Railways, Port and Marine.
Registrar General.
Scientific Research, including Meteorological and Statistical Organization.
Treasury.

C

Non-native Administration—Staff, Estimates.
Agriculture, Non-native areas, legislation, general.
Forests, Non-native areas, legislation, general.
Public Health, non-native areas, legislation, general.
Townships, Non-native areas, legislation, general.
Roads, Non-native areas, legislation, general.
Central Roads Board.
Traffic Control.
Standing Departmental Committee (Local Government).
Water, Legislation, general.
Water, Boring in non-native areas.
Water, Irrigation in non-native areas.
Justices of the Peace.
Trade and Information Bureau.
Mining.

D

Annual Report for Colony.
Blue Book.
Budget and Supplementary Estimates.
Consular and Consular.
Government Press.
Legislative Council.

D. — (Contd.)

- Library
- Official Gazette
- Preparation of Returns for Books of Reference
- Select Committees, secretarial work

E.

- Terms of Service
- Establishment work, including leave, pay, and all matters concerning personnel

F. Local Government —

- General Legislation
- Municipal By-Laws
- Municipalities
- District Councils
- Finance, Audit, etc.

Municipal Finance: —

- Estimates
- Loans, Audit
- Taxation
- Staff
- Government
- Grants

Native Affairs in Urban Areas

Board of Health — Land Sub-divisions

- Municipal Engineering — General — regulations, Municipal water supplies, drainage and sewerage schemes

Town Planning — General

H.

- Greater Nairobi Town Planning Authority
- Land Bank
- Land Settlement — Kenya Advisory Committee
- Land Administration, Nairobi
- Land Administration, Non-native Areas — General
- Survey and Titles

I. Provincial Administration, Staff, Estimates, Postings

Local Native Councils

Native Affairs

- Agriculture in Native Reserves
- Forests in Native Reserves
- Livestock in Native Reserves
- Lands in Native Reserves
- Public Health in Native Reserves
- Roads in Native Reserves
- Water Supplies in Native Reserves
- Water Boring in Native Reserves

Native Labour

Native Registration

KENYA



GOVERNMENT HOUSE
NAIROBI
KENYA

No. 297

REC-21
21 June 1928

24 June, 1928

1st Supplementary
Estimates.

Statement
Re: 1st

Sir,

I have the honour to transmit for your consideration the 1st Supplementary Estimates for 1928, as passed by the Legislative Council on the 19th May, 1928, together with a Statement of certain sums re-voted from a expended balances 1927-1928.

2. A motion was made that the 1st Supplementary Estimates and Re-votes were put forward in two separate resolutions in order that it might be made clear to Council what sums it was asked to vote for entirely new purposes and what sums represented expenditure which had already been approved.

3. A further difference in procedure was adopted by the consideration of Supplementary Estimates by a Select Committee of Council whose constitution was the same as that which was appointed for the consideration of the Budget proposals. A copy of this

Report of Select
Committee.

See also 20 July 1928

-Select-

THE RIGHT HONOURABLE
LIEUTENANT-COLONEL L.O.M.S. AMERY, P.C., M.P.,
SECRETARY OF STATE FOR THE COLONIES,
DOWNING STREET,
LONDON, S.W.

Select Committee's report is attached.

Of the total amount voted for Supplementary Estimates, viz: £190,642, savings are anticipated to amount to £111,052, leaving a net excess of £79,790 on account of urgent requirements which could not be fully foreseen at the time of the preparation of the budget. This sum will be met from Surplus Balances which can be confidently anticipated in respect of this year.

4. I have certain observations to make in addition to the explanations of the various items given in the memorandum appended to these Estimates.

A. You will observe that it was originally intended to make supplementary provision in respect of the Supply and Transport Services. In anticipation, however, of your concurrence in the proposals contained in my Confidential Despatch No. 41 of May 22nd it was decided to delete provision for the time being and consider in connection with 2nd Supplementary Estimates such adjustments as are later found to be necessary after the transfer of control has been effected: together with the provision which will be required for the organization of the first line transport

to which I referred in the 2nd. paragraph of the despatch quoted above.

B. Payment to Nairobi Corporation in respect of Asiatic Reservoir Area - 47,933.

For some years the opening up of further areas in Nairobi, which would be available for Asiatic occupation in order to relieve the congestion in the Bazaar area has been under discussion as regards the size of plots to be sold and the sanitation of the area. In 1926 a sale was held and, in order to bring the properties within the reach of the average purchaser, Government agreed to fix the upset price per plot at the proportionate figure of cost of the necessary roads and drains and to devote that sum in each case to the cost of these services.

The lay-out of the area involved also the construction of a road which it was agreed should properly be treated as a trunk road and Government agreed to meet the whole cost of construction in this case, subject to adjustment later with the Nairobi Municipal Council on the lines recommended by the Local Government Commission, should these be ultimately accepted.

As you are aware, under those recommendations the cost of construction of such a road would fall to the amount of one-half on Government and one-half on Municipal funds. It is probable-

probable therefore that a reimbursement of £1805 in this respect will eventually be obtained from the Nairobi Municipality.

C. Nairobi Town Planning Scheme £3,000.

X
10273/21

This item was referred to in telegram No. 237 of 11/12/20 and in your reply dated December 14/20. Arrangements have been made to utilise the services of the Nairobi Municipal Council Staff strengthened by specific appointments for town planning purposes. The Authority's proposals are now under detailed preparation and it is anticipated that a Plan for the first section, which is being dealt with, will shortly be ready for submission to Government. While it is understood that your approval to this expenditure does not necessarily imply approval of a loan for Town Planning and, accordingly, provision for final expenditure has been made, the sum in question should eventually form part of such expenditure on Town Planning as is approved and is ultimately no doubt to be recoverable.

Acquisition of Section 1st Plot 2575.

This matter is fully dealt with in my despatch No. 255 of May 22nd to which your reply in due course is awaited.

15265
1921

H. Assisted Immigration of Red Legged Laying Stock - £3,000.

In this connection I would refer to my despatch No. 72 of February 6th, with which

was transmitted a scheme prepared by the Acting Director of Agriculture. I have now received your despatch No. 306 of April 27th in which you informed me that the Empire Marketing Board had approved the scheme in principle. Certain slight modifications in that scheme have been intimated in my despatch No. 357 of May 22nd.

15/20
28

Donation to Uganda on Account of Trypanosomiasis Research, 1909.

The proposals of the Uganda Government, to which reference was made in paragraph 6 of Mr. Crasby Gore's despatch No. 649 of July 27th, 1927, have been considered by this Government, which intimated in reply, through the Governors' Conference Secretariat, that it was not at present possible to state the view of this Government with any finality until three questions are settled, viz:-

- (a) The extent to which human and animal trypanosomiasis research should be combined in one Institute at Entebbe or elsewhere;
- (b) The principles on which Group Research Institutes in East Africa are to be financed;
- (c) The principles on which research institutions should be directed and conducted.

The first question has been referred to the Civil Research Committee in England, the second awaits the report of the Hilton-Young Commission, and the third is under consideration by the Colonial Research Committee.

This Government was, in these circumstances, not prepared to express any opinions either on the question of organisation

or on the basis on which the cost of the Institute at Entebbe should be allocated between the different territories; neither is it able to associate itself, at present, with any permanent scheme for Trypanosomiasis Research.

Pending the settlement of the main question referred to above and without necessarily committing this Government to an annual subscription, I agreed to a donation of £900 being made towards the recurrent expenditure incurred by the Uganda Government on this branch of research in 1928.

I intimated to the Uganda Government that in the opinion of this Government the capital expenditure involved should not be apportioned until the whole policy of Research in East Africa has been determined.

G. PUBLIC WORKS RECRUITMENT, Personal Enrolments, 31863.

This matter was reported to you in Kenya despatch No. 142 of March 12th last.

H. Technical Training of African Artizans, £3,160.

I enclose in this connection, for your information, a copy of departmental instructions issued by the Director of Public Works to his Executive Engineers.

Call Webster

*14257
14252*

Departmental Instructions

[Handwritten signatures and initials]

I. Additional Buildings for the Secretariat, Nairobi.

These buildings were required to accommodate the new Local Government Lands and Settlement Branch of the Secretariat, which is now part of the Secretariat, with consequent unification of control. As you are aware, the Commissioner of Local Government, Lands and Settlement, in addition to the functions attached to his office, is also in administrative control, under the Colonial Secretary, of all matters concerning non-native areas - Vide Secretariat Circular No.1 of 1928 - Sections "O", "P", "Q" and "H" - copy of which is also attached.

Circular No.1 of 1928.

Gh. 1529 vol. 2

J. Nairobi Elementary School, £20,000.

In this connection I would refer to the Report of the Select Committee on 1928 Estimates, p.17, in which they recommended the construction, out of loan funds, of three elementary schools in the Nairobi area. It appeared that such provision could only be made from approved Loan Schedules by reducing the sums allotted for the Kabete School. It was therefore agreed to build these elementary schools out of Revenue. The first estimate of £20,000 was reduced to £15,000 as it was considered that slightly smaller schools than at first contemplated would be sufficient and that expenditure may be kept within the amount now provided.

RECEIVED
SECRETARY
GENERAL
1928

K. Drainage of the Peat Swamp Rs. 2,000.

The proposed drainage of the Peat Swamp in the Kalkipia District at the expense of the riparian land-owners so as to improve the water supply and reclaim a considerable portion of the Swamp for agricultural purposes has been under discussion some years, but has hitherto been rendered impracticable by the refusal of certain of the riparian landowners to consent. It became possible in January 1906 to proceed with the scheme and as the Commissioner of Lands advised that the reserved land could be disposed of with considerable profit to Government, I directed the Director of Public Works to put the work in hand forthwith.

5. I trust that these estimates will meet with your approval and that I may be so informed by telegraph.

I have the honour to be,
Sir,

Your most obedient, humble servant,

Wm. D. Sullivan

GOVERNOR'S DEPUTY.

REPORT OF THE SELECT COMMITTEE ON THE 1st SUPPLEMENTARY ESTIMATES, 1928, AND THE STATEMENT OF THE ACCOUNTS PAID FOR TO BE REVOKED IN 1928 FROM THE UNRECORDED BALANCES OF 1927 VOTES.

Head VIIIn - Administration Extraordinary.

Swaine Relief, Barings District, £500.

The Chief Native Commissioner undertook to draft and to lay on the table of the Council a statement in regard to this matter.

Head XI - Military £15,445.

It was agreed that the items to this amount should be deleted. The Committee expressed the hope that the transfer of the Transport to the Public Works Department could be carried out as soon as possible.

Head XVII - Miscellaneous Services.

Commission and Fees to Crown Agents, £500.

Attention was drawn to the figure of 1% given in the Explanatory Statement as the amount charged by the Crown Agents for the management of the £5,000,000 loan. The Treasurer gave an undertaking to have this figure verified.

Assisted Importation of Pedigree Livestock, £23,000.

In the last two lines of the Explanatory memorandum under the head of the words "It is, however, provided that only one assisted freight is allotted in each class of stock to any one applicant" were ~~xxx~~ deleted.

Acquisition of House and Land in the vicinity of Elmore School, £4,000.

It was agreed to insert this amount as an additional item under Head XVII.

Head XXI - Public Works Extraordinary.

Quarters for African Apprentices of the Medical Training Schools, Nairobi £950.

The Hon. J. O'Shea asked that Government would consider a scheme for training European Nurses. An assurance was given of sympathetic consideration of such a scheme.

Nairobi Elementary Schools, £20,000.

It was agreed to reduce the provision under this item to £15,000.

Roads and Bridges, £100.

It was decided to delete this item.

(signed) E. J. DUNN,

CHAIRMAN,
for Committee.

P.O. Box 662.

PUBLIC WORKS DEPARTMENT,

HEAD OFFICE,

NAIROBI.

5th April, 1928.

CIRCULAR NO. 37.

Executive Engineer,	Mombasa,
"	Nairobi,
"	Alders,
Ag. "	Kisumu,
"	Nakuru,
Assistant	Nyeri,
"	Kitale, through Executive Engineer, Eldoret.

AFRICAN ARTIZANS.

Government has decided that much more vigorous steps are to be taken by this Department to train Africans to become artizans than in the past. It will be understood that Government, and I myself also, regard this matter as being of profound importance. Executive Engineers and all technical officers are enjoined to give, and to continue to give, this matter their most careful attention, even though the results may for the first few years be disappointing.

2. There are several methods in operation at present for training African artizans:-

(1) Native Industrial Training Dept., Kabete.

At this institution training is carried out more or less under school conditions, the youths being indentured to particular trades for 5 years, or less if the boy has already acquired some proficiency on account of prior training at a Mission school or elsewhere. The depot has accommodation for 300 apprentices under training at the depot and aims at having 300 more at work.

on Public Works in the Colony in the future.

About 64 are at present at work on buildings for the Public Works Department, mostly in Nairobi.

The method of training is through the medium of European leading Artizans. The wages paid to the pupils are 4/-, 5/-, 6/-, 8/- and 10/- per month in successive years during the period of apprenticeship.

(2) The method adopted by the Prisons Department is similar to that of the Native Industrial Training Depot in that European instructors are employed to teach the prisoners. A gang of prisoners employed in the Joinery Shop at the Public Works Department, Nairobi, is reported by the foreman to be doing good work.

(3) Railway.

Two methods are employed by the Railway:

(a) The Apprenticeship System.

At present there are 154 boys indentured for 5 years, according to an agreement which requires teaching on similar lines to the Native Industrial Training Depot agreement. Teaching is given in the workshops and also at the hostel after hours. The teaching at the hostel is partly literary. The wages paid are higher than at the Native Industrial Training Depot, being 6/-, 8/-, 12/-, 16/- and 20/- per month in successive years.

(b) The Artizan System.

A large number of native artizans are employed by the Railway in their workshops. They are graded as follows: 1st class artizans 20/- to 40/- per month; 2nd class artizans 15/- to 30/- per month; 3rd class artizans 10/- to 120/- per month. There are only two 1st class artizans at present. 2nd and 3rd class artizans get ration. The method of recruitment is merely the selection of natives of intelligence from labourers. They are not on agreement. The method adopted for training and improvement is merely to make them work alongside trained artizans or any man who assist them with their work. Their increase of pay depend on their progress. A large proportion of the work in the Railway workshops is now done by these native artizans. The Chief Mechanical Engineer of the Kenya and Uganda Railway regards this system as yielding better results than the method described in (a) above. Difficulty has, however, been experienced through artizans getting tired of their work and leaving after having progressed some way.

(c) Public Works.

In October, 1937, the Department was employing 49 native artizans (other than motor drivers) at rates varying from 13 cents to 61 cents per hour.

(5) Private Enterprise.

Some motor firms, farmers and a few contractors

(especially in respect of painters) have had a fair measure of success by adoption of the method outlined in (3) (b) above.

3. The system which is now intended to work in this Department is a modification of (3) (b) above. Natives who have been employed in the Department or otherwise for some time and who have shown a good degree of intelligence, aptitude and reliability during their work should be selected. In every division there are natives of this type who are more intelligent than the average and who are more or less reliable in respect of such operations as lining out, mixing concrete, fixing fencing, making huts, helping artisans, simple repairs to motor vehicles, etc. These natives may be regarded as having had initial mental training in work. Moreover, they are often to some extent detribalised and more likely to desire permanent employment in trades which offer higher wages as proficiency progresses. When enrolled as artisans they will, of course, work the ordinary working hours of the Division. Natives who have received initial training at mission schools or the Government technical school should not be enrolled as Government does not desire competition with the Native Industrial Training Depot as a finishing school.

4. The grading of the native artisans will be as follows:-

Fourth Class Native Artisans	16/- to 20/-	per month
Third	22/- to 40/-	"
Second	42/- to 60/-	"
First	32 cents to 62 cents	per hour.

INDUSTRIAL TRAINING DEPOT

Ordinarily a selected labourer will enter as a fourth class artisan at the wage which he is obtaining, or would obtain if he were employed, as a labourer. If, however, the wage which the boy is obtaining in some capacity other than labourer is between 20% and 40%, he would be enrolled in that grade at the wage which he is obtaining. Native artisans who are at present employed at a rate per hour may either be enrolled under this scheme or remain as casual employees at an hourly rate at the discretion of the Executive Engineer. I do not, however, wish any artisan to be enrolled as a first class artisan, nor should any untrained artisan enter in any grade above the third and only in exceptional cases should an untrained artisan commence higher than the bottom of this grade. If there are any native artisans at present who are really proficient, I prefer that they be designated "leading native artisans" and their work largely that of instruction in the use of tools. When work is required at out-stations, a temporary increase in wages not exceeding 25% may, in special cases, be given while so employed, at the discretion of the Executive Engineer.

5. I shall expect all European foremen and Indian sub-foremen to take an active interest in the scheme and to co-operate fully in making it as successful as possible. It has been stated that

those of native labourers. Accommodation separate from those of ordinary labourers should be provided for them. When employed away from the base, they will ordinarily have either portable corrugated iron shelters or tents.

8. It is desired to aim at having at least 150 native artisans in the Department. It is recognised that the selection of natives of the proper type suitable for training under this scheme cannot be effected immediately in large numbers. The number to be aimed at in each division must be left largely to the discretion of Executive Engineers.

9. The number for each trade must be left indefinite at present and be dependent on requirements in each division. The chief demand is likely to be for builders in concrete block and brickwork, plasterers and carpenters.

10. Arrangements should be made in each case for instruction in the use of tools, in the first instance, by a skilled artisan. There is a sum in the estimates this year to cover initial expenses, such as purchase of tools, but, after the boys have received such initial training, they should be employed on works with trained men as above described and their wages and other expenses charged to those works. It is not intended to give any literary or other training in schools or depots, though initial instruction in the reading of the

For rules should be given. It has been decided by Government that this system of training is only to be adopted if there is an European in charge who is competent in the trades in which the artizans are acquiring knowledge. It is intended to engage six Europeans for the purpose who will be distributed as required after engagement. Meantime the scheme is only applicable to Loan Buildings, where it is impossible in divisions to give the initial training in the use of tools which is required, such natives as enter the scheme should be sent to Nairobi in the first instance.

11. It has been decided by Government that Indians may be employed on those Class E buildings of the Loan Schedule where this method of training Africans is adopted.

(Signed) H.L. SIKES.

DIRECTOR OF PUBLIC WORKS

COLONY AND PROTECTORATE OF KENYA

COLONY AND PROTECTORATE OF KENYA

PUBLIC WORKS DEPARTMENT

PUBLIC WORKS DEPARTMENT

MAPATANO yamefanyika siku

AN AGREEMENT made the

ya ... ya mwaka wa

day of ... 19

KATIKA Director of Public Works (ambao hapa yulitwa "Mwenyi Kazi") kwa upande mmoja na

... the Director of Public Works (hereinafter called "the Employer") of the one part and

kipande cha namba ... Mwasiti

... registered number

(ambao hapa yulitwa "Fundu") kwa upande mwingine

(hereinafter called "The Artisan") of the other part

KWA KUWA huyu Fundu wamesikizana na huyo Mwenyi Kazi nao, ane jifunga mwenyewe kwa kumtumikia katika Colony and Protectorate of Kenya kwa muda wa miaka ... kuhesabiwa tangu ...

WHEREAS the Artisan hereby contracts with the Employer and binds himself to serve ... the Colony and Protectorate of Kenya for a period of ... years to be computed from ... in the capacity of ...

Katika Kazi ya Fundu wa ... kwa mkataba huu ufuatanao

Artisan upon the terms and conditions hereinafter specified.

MAPATANO YENYWE NI KAMI HINA

IT IS HEREBY AGREED as follows:

Fundu apatana nao ajifunga mwenyewe kama hivi:-

The Artisan agrees and binds himself as follows:-

- 1. Kwamba, banda ya kumaliza miezi mitatu ya kujanibwa na kazi yako, likionskana kuwani, nzuri kwa huyo Mwenyi Kazi, au kwa muda wowote apandao yule Mwenyi Kazi, nao kwa hidi na kwa aminifu na kwa uwoto wakwa tucto atatii amri zote na kufanya

- 1. That, having completed a probationary period of three months to the satisfaction of the Employer, or such longer period as the Employer may regard as necessary, he will diligently, faithfully, and to the best of his ability obey all

Katika kuandikwa kwa huo huo
 Fundi atapawa blanketi na
 atalipwa kwa kadiri ya mshahara
 wa Grass atakarokua. Na mara kwa mara
 huyo Mwenyi Kazi atamru
 mishahara longezwa na huyo Fundi
 aendelea apandelewe katika
 darasa ya juu. Na amapo Fundi
 nafai katika mafunzo yakw au
 akiwa mviya au akiwa haendelei
 Mwenyi Kazi atamrudisha darasa
 la viki au ampa mshahara
 wakw au vyovyote atakavyofanya
 kwenyewe akiona ni vyema. Na
 zaidi ya mshahara huo uliotajwa
 hapo juu Fundi atapawa na posho
 kama vile wapatavyo wengine
 wafanyao kazi Public Works
 Department. Mishahara itakatwa
 akiwa mtu amechelewa au
 asipotii mri au akiya
 ameharibu mali ya Mwenyi Kazi
 au mshahara wote utazuliwa iwapo
 mtu amegua ugonjwa wa kasi kama
 wenyewe aliwahi kwa india ya
 kuhujitunza kwakw. Mtu
 akiwa hakuja kazini kwa ugonjwa
 wa haki atapawa kama nusu ya
 mshahara wakw muda wa majuma
 matatu ya ugonjwa baada ya hapo
 hatapawa mshahara tena mpaka

Upon engagement the Artisan will
 be given a blanket and will be
 paid at the initial wage of the
 class which his proficiency
 entitles him to. From time to time the
 Employer will sanction increases
 of pay and transfer to a higher
 class strictly in accordance with
 the Artisan's progress and
 efficiency. In the event of the
 Artisan proving inefficient or
 lazy or failing to make progress
 the Employer may reduce him in
 grade or pay to whatever extent
 may seem to him suitable. In
 addition to the above wages the
 Artisan will be given rations on
 a similar scale to other natives
 in the employ of the Public Works
 Department. The wages will be
 subject to partial stoppages in
 the event of unpunctuality,
 insubordination or wilful
 destruction of Employer's property
 or full stoppage for illness due
 to his own neglect or misconduct.
 In the case of absence from work
 owing to illness not due to his
 own neglect or misconduct the
 Artisan will be paid at a rate not
 less than half his ordinary wage.

amudi kazini.

3. Huyo Mwenyi Kazi atampa huyo Fundi vyombo vyaavyo kwa hiyo kazi yakwe nae Fundi atavitwaa hivyo vyombo kwa uraka yakwe vikipotea au akivitumia vibaya itamlazimu kuviliplia thamani yakwe vote au fungu la thamani yakwe kama atakavyofikiri huyo Mwenyi Kazi. Huda wote utakapomalizika wa miaka

..... na itakapokuwa kabisa ya huyo Fundi imeonakaha kuwa ni nzuri ndo ameonyesha bidii na uhodari katika kazi yakwe, huyo Mwenyi Kazi atampa hivyo vyombo vyote alivyokuwa akivitumia na ambavyo ndiyo hasa vya kazi ya ufundi hamaa hivyo awa navyo kabisa.

4. Mwenyi Kazi atampa huyo Fundi ruksa ya majuma matatu kwa kila mwaka na mshahara mzima nae atalipa gharama ya Railway ya kwenda na kurudi

for a period not exceeding three weeks at a time after which he will not be paid until he resumes work.

5. The Employer will provide the Artisan with suitable tools for his trade and the Artisan will be responsible for all tools placed in his charge and loss or misuse of same will involve payment of their value either in whole or in part as the Employer may decide. Upon completion of the whole period of..... years, and provided that the conduct of the Artisan has been good and he has shown diligence and efficiency in his work, the Employer will give him the ordinary set of tools he has been using and which it is customary for an Artisan of his trade to possess.

6. The Employer will give the Artisan three weeks leave in every year, which shall not be accumulative, on full pay and will pay his return railway fare to the Railway Station nearest his home.



COLONY AND PROTECTORATE OF KENYA

**FIRST SUPPLEMENTARY ESTIMATES,
1928**

First Supplementary Estimates, 1928.

No. of Head	Head	Sub-Head	Sanctioned Estimates, 1928	Amount approved by resolution in Legislative Councils	Amended Supply Estimate, 1928	Expected Savings
	PUBLIC DEBT FUNDED	Interest Ordinance No. 22 of 1927 Loan £2,000,000 @ 8% Kenya and Uganda Railway	£	£	£	£
					125,000	
III.	H. E. THE GOVERNOR	Personal Emoluments	11,638		101	
IV.	H. E. THE GOVERNOR EXTRA-ORDINARY.	Purchase of Motor Vehicles			275	
VII.	PRINTING AND STATIONERY.		39,277			1,187
VIII.	ADMINISTRATION, EXTRA-ORDINARY.	Motor Cars, Kyambui	250		110	
		Famine Relief, Baringo District			500	
			250		610	
X.	TREASURY.	Personal Emoluments	22,507		209	
XV.	LEGAL DEPARTMENT.		10,340			476
XVI.	POLICE.		148,864			5,600
XVII.	PRISONS.	Personal Emoluments	20,422		905	
XVIII.	MEDICAL DEPARTMENT.		204,801			4,240
XX.	MILITARY.	Supply and Transport Services K. A. R.			8,800	
		Personal Emoluments	1,850			
		Rations	200		637	
		Clothing			150	
		Travelling Allowances			100	
		Local Transport and Travelling			1,200	
		Passages	150		181	
		Upkeep of Animal Transport			180	
		Upkeep of operation by M. T.			4,200	
		Loss of Stores			20	
			2,200		15,446	
XXII.	AGRICULTURAL DEPARTMENT		144,418			1,500
XXIII.	FOREST DEPARTMENT.		37,755			211
XXIV.	GAME DEPARTMENT.		10,556			265
XXVII.	MISCELLANEOUS SERVICES.	Purchase of Cash Safes and Boxes	600		600	
		Commission and Fees to Crown Agents	2,200		500	
		Development of Aviation			400	
		Payment to Nairobi Corporation in respect of Asiatic Residential Areas			933	
		Nairobi Town Planning Scheme			1,000	
		Expenses of Hilton Young Commission			650	
		K. & U. R. garzoon of Stand Premises and Rents			12,000	
			2,800		23,223	
		<i>Carried forward £</i>			42,441	12,584



COLONY AND PROTECTORATE OF KENYA

AMOUNTS APPLIED FOR TO BE
REVOTED IN 1928
FROM UNEXPENDED BALANCES
OF 1927 VOTES

101

EXPLANATORY MEMORANDUM OF AMOUNTS APPLIED FOR
TO BE REVEALED IN THE FROM UNEXPENDED
BALANCES OF 1927 VOTED

ADMINISTRATION EXTRAORDINARY

Motor Cars and Trucks, £15.—In 1927 some Motor Cars were ordered for the use of the Police but the order was not executed until 1928 and the bills for the same remained due to the payment for part of the purchase of these articles.

EDUCATION DEPARTMENT EXTRAORDINARY

Nairobi Industrial Training Dept. Equipment, £103.—Towards the end of the year 1927 a second hand Ford Motor Lorry was purchased but as it was not paid for before the close of the year, this amount is now required to meet the purchase.

POST OFFICE AND TELEGRAPHS EXTRAORDINARY

Replacement of Nairobi Telephone Exchange by an Automatic System, £8,417.—The unexpended balance of the amount voted in the 1st Supplementary Estimates, 1927, is required for completing the work.

Local Telephone Cable, £120.—The unexpended balance of the amount voted in the 1927 Estimates is required for completing the work.

AGRICULTURAL DEPARTMENT EXTRAORDINARY

Expenses in connection with Muiwa State Borer Investigations, £566.—The unexpended balance of the amount voted in the 2nd Supplementary Estimates, 1927, is required for completing the investigations.

MISCELLANEOUS SERVICES

Bounties of Planting, Islands at Kisumu, £346.—The unexpended balance of the amounts voted in the 2nd Supplementary Estimates, 1927, is required for completing the work.

Removal of Nairobi Race Course, £341.—The unexpended balance of the expenditure approved by motion of the Legislative Council in the 1927 March, 1927, is required for continuing the work.

Portraits of Previous Governors, £129.—In the 1927 winter was placed in England for the painting of eight portraits of Kenya Governors but as the order was not executed until 1928 the unspent balance of the amount voted in the 1927 Estimates will be required for meeting charges during 1928.

PUBLIC WORKS EXTRAORDINARY

New Press Buildings, £408.—The unexpended balance is required for completing the work.

Postal Buildings, £1,150.—The unexpended balance is required for completing the erection of the buildings which were in the process of being erected at the close of the year.

Police Stations, £2,325.—The unexpended balance is required for completing the erection of the buildings which were in the process of being erected at the close of the year.

Timber Seasoning Plants, £2,690.—The unexpended balance is required for providing apparatus for the timber seasoning kilns.

Temporary Buildings for Nairobi Police Post, £120.—This amount was provided in the 2nd Supplementary Estimates, 1927, but the work was not started before the close of the year.

Bungalows for Foresters, £74.—The unexpended balance of the amount voted in the 2nd Supplementary Estimates, 1927, is required to meet part of the cost of completing the bungalows for Foresters at Pasi, Karigat and Mount Elgon.

Establishment of a Plant Breeding Station at Njoro, £3,121.—The unexpended balance of the expenditure approved by motion in Legislative Council on the 12th of March, 1927, is required to complete the work.

Roads and Bridges, £1,209.—This unexpended balance is required to complete the following works which were started in 1927 but not completed before the close of the year:

Number of Roads	Amount
Road between Bank and Cash Stores, District, Mumias, Mombasa	11
Completion of bridge over Nyveris, Kisumu-Kisii Road	282
Bridge over Mungoni River, Nairobi-Kyambisi Road	324
Reinforcement to Tully's Fort Hill Road	789
Nairobi and Mombasa, Malindi, continuation of construction and installation	1,800
New road from Soloi Road to S.S. Road No. 2, etc.	852
Bridge over Soko River, Karsabet Nakuru to Mumias Road	600
Bridge over Lamuna River, Kakamega-Uruga Road	817
Continuation and improvement of existing tracks and replacement of temporary and semi-permanent bridges at Eldoret and Transvaal, etc.	867
Continuation of roads and bridges to Railway Station, Nyeri	147
Bridge over the Kasu, Nyeri River, Mumuru-Nanyuki	238
Roads and bridges, etc.	801
Bridge over Bio River, Mumuru Road	1,411
Miscellaneous works	1,209
	(£10,200)

Kochereba-Lollear Road, £1,089.—This unexpended balance is required for completing the re-alignment of 23 miles of this road which was insuitable for motor transport.

Drainage on Mpinga River, £765.—This unexpended balance is required for finishing the conversion of the temporary bridge into a permanent structure.

Nukuru School Drainage System, £1,150.—This amount was voted in the 2nd Supplementary Estimates, 1927, but the work had not been started at the close of the year. The vote is to provide new washing, bathing and latrine accommodation, also for the reconstruction of the drainage system at Nukuru School.

Furniture for New Buildings, £4,516.—The unexpended balance is required for supplying furniture for new buildings.

likely would be sent. An action was to send
copy to the press, without any specific
reference to the hotel, etc.

This experience is that in any disputed case
where the conduct is debatable, should special
consideration of the travel, the fact of having
indicating how to regard the case. It seems that
certain action may be taken, if not in dispute
where we get no idea of that kind. It is
concluded that it is better to have
indicating that it is better to have
than in this case would be suitable. I think
that the only action would be to have
copy of the fact of the case.

July 14/28

I have discussed it with Mr. Brown. I think
that it is better to have
the 92^d minima to make of our getting
ambiguity reports of the kind as follows
to

Orthman
12.5.28

To Mr. Givens (Sgt) 15 Aug 28

6 Foreign Office 11 July 1928
Lords copy not in Foreign Office
has opinion of King and that has been
notified of the Foreign Office as a record

Mr. Givens
E. Givens

7/8 above

Mr. Givens
12/10/28

Mr. Givens

12/10/28

Mr. Brown

Please see note in unsc. (3) Mr.

James has done nothing I will have
written for this copy action by 7.8
necessary

12/10/28

Mr. Givens to Mr. Brown (Sgt) 16 Sept. 28
dis. generally of opinion that no. 5 is quite
reasonable that expenditure of Government
entertainment allowance must be kept within
moderate limits

Copy sent to
reg'd. 5097/28
General

Copy to (S) 5(7) has letter to
for mail. It is to be published
12/10/28

Lk
Alan
2/1/28
A Y
FOREIGN OFFICE, S.W. 1.

11th September, 1928

(L. 5198/4057/405)

My dear Bevir

no
I have circulated throughout the office your letter No. X 15284/23/Kebya of August 15th for "information and guidance". We are generally of opinion that it is quite reasonable, a small proportion of the entertainment allowance which Colonial Governors are provided seems to us well spent on the entertainment of duly recommended foreigners, but we quite realise that such expenditure must be kept within moderate limits.

Yours sincerely

Stephen Gascolee

Anthony Bevir, Esq.

10th July 1928.

6

Your Excellency,

With reference to Your Excellency's Note No. 50
of the 20th ultimo, I have the honour to inform you that
the Secretary of State for the Colonies has forwarded to
the Governors of Tanganyika Territory and Kenya the
notification of Monsieur Kroglinger's visit, as desired.

I have the honour to be, with the highest
consideration,

Your Excellency's obedient servant,
(For the Secretary of State)

(Signed) S. GASELEE.

- Mr. Bevir
- Mr. Williams 15.8.28
- Mr. Bevin
- Mr. B. Callaghan
- Mr. D. Sturges
- Mr. J. Strickland
- Mr. C. G. G. G.
- Mr. G. D. G.
- Mr. B. Wilson
- Mr. Ormsby-Gore
- Lord Lovat
- Mr. G. G. G.

Downing Street,

16th August, 1928.

My dear Caselee,

You may remember that I spoke

and wrote to you some weeks ago about
the visit of M. Kreglinger to Kenya
and Tanganyika.

DRAFT for *circulation*

CASSELEE, ESQ., C.I.B.S.

You may have noticed that we

passed on the letter from the Belgian
Embassy without comment. As a matter
of fact we are generally very reluctant
to give any form of introduction,
except of course for those on official
business, to Colonial Governors for
the following reasons.

In many cases any communication
about the visit of an individual from
the Colonial Office is apt to be taken
as necessitating an invitation to stay
at Government House; - certainly an
invitation to a meal. In many Colonies

*Rec'd to Mr. Bevir
at the Dept. of
the Secretary
[after giving Mr. G. G. G. G.
his comments on the invitation]*

where

and District Officers know, and these

again feel that it is incumbent upon

them to offer hospitality which they

may be able to perform

I hope, therefore, that you

will answer and that there was peace

was not making any answer at all

what is to be done in this production,

or giving

to be a help in

communicating with

the individual

concerning to the

salary and if there is any real need

to pay part of the hospitality

we feel that it is

the local people to let

them know beforehand what to expect

and what, if anything, they are expected

to do, especially when it may involve

them in no inconsiderable expense.

*Yours sincerely
Sgt. Anthony Davis*

Immediate

In any further communication on this subject, please quote
No. *L405/1405/1405*
and address *not to any form of postal order*
The Under Secretary of State, Foreign Office, London, S.W.

RECEIVED
29 JUN 1928
FOI OFFICE

THE Under Secretary of State for Foreign Affairs presents his
compliments to *the Under Secretary of State for*
the Colonies and in transmitting herewith, by direction
of the Secretary of State, copy of the under-mentioned paper, would
be glad to be advised what answer should be returned to *himself*

Foreign Office
June 27th 1928

Reference to previous correspondence

Description of Enclosure.

Name and Date:	Subject:
<i>Fran Belgian Embassy</i> <i>of June 26 1928</i> <i>5 JUL 1928</i>	<i>Facilities for oil trucking</i> <i>in Kenya and Tanganyika</i>

Ans. 4-7-28
Copy sent to Kenya 4-7-28
7-7-28

Similar letter sent to

Foreign Office 2 July 1928
States as to representations of Prince Bismarck in
regard to case of Fritz Kloppe who was sentenced
to three years imprisonment for inflicting grievous
bodily harm, followed by death, on a woman, inquisitor
whether any information is available for transmission to
the German Embassy

Mr. [Name] spoke to me about this &
submit file for [Name]

[Name]

The file of Prince Bismarck

mentioned to go to Mr. [Name]

[Name]
[Name]

to [Name] will [Name] [Name]

The enclosures to W. 2. have not yet
been returned I remain [Name]

[Name]

I am [Name]

[Name]

[Name]

[Name]

Foreign Office 30 July 1928
has copy letter to German Embassy
replying to Prince Bismarck's representation

not necessary to send copy [Name] to [Name]

[Name]

A) ...
B) ...

copy of ...
was to ...
is

stroke
1/1

at a ...
all

fresh Pass cutting
(on X 1506 ... (18) ...)

submit ...
copy of this ... also to Gov. Lt.
(without encs)

3/8

all
1/1

To Gov. ...
(to ...)

To Gov. ...
1, 2, 3 ...

1/1

Foreign Office, 16th August 1928.
Return Press cutting enclosed with ...

Put by
(Sd) J. O. EASTWOOD
30/B.
at once

Foreign Office, 18 Oct. 28.
Tsd. translation of an "atque-memoire" from the
German Charge d'Affaires, regarding the case of
Fritz Klepper and the possibility of his being
granted a pardon.

This is a request for the exercise of the
Prerogative and it is a well established principle
that the Prerogative is for the Governor's sole
discretion and the S. of S. does not intervene.

(Intd) A.S.C.P.
Certainly ...
of the ...

See Sir E. Rieley's memo in Nigeria 33945/24
on the general question and as regards particular
cases (a) Mr. Ormsby Gore's minute on No. 14 of
35298/27 W.P. (b) the minutes by Sir G. Chindle
and Sir G. Riddge on 67902/19 W.I. and 34895/21
W.I. respectively. There are of course numerous
other precedents.

In reply that the Royal Prerogative of
mercy has in Kenya been constitutionally delegated
by Letters Patent to the Governor and it is a well
settled practice that the S. of S. will not interfere
with its exercise by the Governor. The S. of S.
can therefore take no action in the matter beyond
sending a copy of the correspondence to the Governor
for his information and this is being done.

Clause XIX
of I.P. 11.9.20

and

In any further communication on this subject, please quote

No. **C 8283/5041/18**

and address ~~not to any person by name~~, but to

The Under Secretary of State, Foreign Office, London, S.W.1.

The Under Secretary of State for Foreign Affairs presents his compliments to the Under Secretary of State for War and by direction of the Secretary of State transmits herewith copy of the under-mentioned paper.

Foreign Office

November 12, 1928.

Reference to previous correspondence

15287/28

November 12, 1928

Description of Enclosure.

Name and Date.

Subject.

To German Charge d'affaires
November 12

Sentence of imprisonment
passed on Fritz Klöpffer
in East Africa

Similar letter sent to

6
Foreign Office, S. P. 1111

10th November, 1920.

10. C 2227/2041/10

Sir,

With reference to the memorandum communicated by you to Mr. Sargent on October 16th last, setting forth certain details relative to the sentence of imprisonment passed on Herr Fritz Klopfer in Kenya Colony and enquiring whether in view thereof the possibility of granting a pardon might not be considered, I have the honour to inform you that the Royal Prerogative of mercy has in Kenya been constitutionally delegated by Letters Patent to the Governor and that it is a well-settled practice that the Secretary of State for the Colonies should not interfere with the exercise of this prerogative by the Governor. It is regretted that in these circumstances it has proved impossible to take any action in the matter other than to communicate copies of the relevant correspondence to the Governor for his information.

I have the honour to be,
with high consideration,

Sir,

Your obedient servant,
(For Lord Cushing)

Monsieur H. H. Dischhoff,

Sir, etc., Sir.

action in the matter other than
to send a copy of the correspon-
dence to the Governor of ~~Ohio~~
for his information, ^{which} ~~this~~ is
accordingly being done.

I am,

etc.

(Signed) J. P. CARKINSON

Captain Schwartz, the defending lawyer, having informed Herr Karlowa that, according to the existing law, the Governor in Council was alone competent to grant a pardon and therefore could do nothing by himself but could only act at the suggestion and in agreement with the Executive Council, the German Consul ~~first~~ approached Sir Edward Denham, the Chief Secretary, who is also a member of the executive council. This took place on June 18th last in company with the defending counsel. Captain Schwartz recounted in detail the facts on which both sentences were based. It appeared from his remarks that the decisive point to determine the whole case was the answer to the question whether, at the moment when Klöpfer threw his stick at the native, the latter had turned his face to Klöpfer or not. Only in the former event was a legal decision at all possible such as the jury had given in the case. The defending counsel demonstrated very clearly to Sir Edward that he had produced conclusive evidence that, at the moment when Klöpfer threw the stick the native still had his face turned away so that it was therefore logically impossible that Klöpfer, when he threw, could have had the intention of inflicting on the native an injury endangering life. Upon Sir Edward stating that it was difficult by a decision of the Governor in Council to mitigate further the sentence of a court of second instance which had made a reduction in the sentence pronounced, Herr Karlowa expressed the opinion that this was an extraordinary case, for it could not be doubted that the jury had here made a mistake and that it had not been possible entirely to obviate, by the decision of the Court of Appeal, the consequences of the mistake because that Court, according to the regulations governing criminal procedure in force in Kenya, could not enter into a discussion whether the jury had rightly appreciated the facts on which the sentence

was based. The German Consul further observed that he considered it his duty to speak quite frankly with Sir Edward and declared that both sentences would be unanimously regarded by his countrymen as wrongful and that he had cause to believe that this view taken by his countrymen was shared in wider circles. The opinion was quite generally held in these circles that the extent of the punishment was in striking contrast to other sentences in cases where a much more incriminating set of facts for the accused justified the adverse decision. He would inform the Foreign Ministry in Berlin of the position and he once more asked that the question whether the sentence of the court of appeal could be reduced, might be submitted to the Governor in Council. Edward then arranged with the defending counsel that the latter should immediately have a conversation with the Solicitor General for the purpose of preparing a submission to the Governor in Council.

The German Consul communicated with the Foreign Ministry, making this report and attaching in the first place an article in the "East African Standard" of June 19th, 1928, which condemned the severity of both the sentences and secondly a letter enclosing a memorandum by the defending counsel, Captain Schwartzke, a copy of which is forwarded with this aide-memoire. The Foreign Ministry transmitted the documents to the German Embassy here requesting them to discuss the matter with the Foreign Office. On June 29th last a member of the German Embassy had a conversation relating to the case with Mr. Huxley at the Foreign Office, who promised a reply after consideration of the matter. This reply, dated July 25th last (C 2341/5941/10), was worded as follows:-

Welsh
to Huxley

Your...

conditions, or any remission of the sentence passed on such offenders, or any respite of the execution of such sentence for such period as the Governor think fit. It may result any loss, penalties or forfeiture, or acc't to Us. Provided always, that the Governor shall in no case, except where the offence has been of a political nature unaccompanied by any other grave crime, make it a condition of any pardon or remission of sentence that the offender shall be banished from the island absent himself or be removed from the island any.

the Honourable Board, with the highest consideration.

"Your Excellency's obedient Servant,
(For the Secretary of State)"

"Edw. C. ..."

"F. R. Monsieur Friedrich Stumel ..."

In virtue of his information the German Consul at ... was instructed to write himself again in communication with the Governor with a view to securing a pardon. He thereupon caused a petition to be forwarded to the Colonial Secretary through Herr Schwartz, the defending counsel. In his reply the Colonial Secretary informed the defending counsel that the Governor saw no ground for taking further action in this matter.

In view of this negative reply the German Government have instructed the Embassy to speak to the Foreign Office again about the case and, having regard to the circumstance that local efforts to obtain a pardon have failed, to discuss whether in view of all the facts, it does not seem desirable to consider the possibility of a pardon being granted.

10
COPY

(C 7891/6041/18)

Anlage zum Bericht des Konsulats Mombasa vom 6. Juli 1922, I. Nr. 1693

Shapley & Schwartz
Advocates & Solicitors

Nairobi, June 22nd 1922.

Ref.No. 48/8/3

Sir,

Re. Mr. Klöpper

His Excellency the Governor interviewed me in connection with the above matter yesterday afternoon when there were present the Colonial Secretary and the Attorney General.

After considerable discussion, His Excellency announced that he would have to read the whole of the papers and also discuss the matter with the Chief Justice of Kenya, who was president of the Court of Appeal.

As soon as I receive an intimation of His Excellency's decision, I will communicate with you further.

Meanwhile, I enclose, as requested, a Memorandum setting out the salient features and history of the case.

I am &c.,

(Signed) H.E. Schwartz

The Consul General for Germany,
Mombasa.

two native witnesses for the defence and the Accused himself in the witness box stated that the boy was running away and turned his head over his right shoulder just at the moment the stick reached him.

The Judge sentenced the Accused to three months rigorous imprisonment.

The appeal was lodged at the Court of Appeal Western Africa, but this appeal, in accordance with provisions of the Criminal Procedure Ordinance obtaining in this Colony, could only be on the grounds of error of law, there being no appeal on a trial by jury.

The Court in considering the question of sentence, were bound to take into consideration the fact that the Accused had been severely and grievously hurt.

It has always appeared to me that the most important aspect of the whole case is as to the position of the wound on the face. It was shown, and I expressed to the jury, and upon the part of almost all the witnesses, the wound was a horizontal one, it was on the right side of the face and it was on the forehead of the accused.

The witness who was asked to show the stick and the position of a blow is thrown forward with the stick in the air, it is possible that a man could be struck on the forehead if he was the only one in the room.

A further demonstration will show that, with a stick held in the right hand, the wound would be on the right side of the face and if the stick is on the left side of the face and if the stick is on the left side of the face.

I am strongly of opinion that the sentence of

six months imprisonment for throwing a fish-bone stick
at a running boy is out of all proportion to the offence,
and I am equally of opinion that the Jury acted very wrongly
in finding the accused guilty of grievous hurt and that to
say that a normal man must have known that such an action
would be likely to endanger life is unsustainable.

The Trial Judge asked the Jury to find as a fact
whether the boy was kicking the accused or running away, but
whether intentionally or unintentionally. I am not sure they
omitted to do so.

I regret to state that, in my opinion, it is
impossible to lose sight of the fact that the Jury were
aware that the accused was a Gorman.

I have had many interviews with Mr. Kilgarriff and
my own personal opinion is that the whole episode was one
of extremely hard luck. That he is by no means a vicious
character, and that he had met with a fate which was quite
undeserved.

(Signed) H.E. Schwitze.

23. 5. 28

Handwritten notes:
This is
the
report
of the
jury
at the
trial
of
the
boy
who
kicked
the
Gorman
and
was
found
guilty
of
grievous
hurt
and
sentenced
to
six
months
imprisonment.
The
jury
was
of
the
opinion
that
the
accused
must
have
known
that
such
an
action
would
be
likely
to
endanger
life.
I
am
not
sure
they
omitted
to
do
so.

196

In any further communication
on this subject, please quote
No. C/6125/8041/18.

Send address
not to any person by name
but to
The Under Secretary of State
Colonial Office
London, E.C.4.

FOREIGN OFFICE

14th August, 1928.

Sir,

With reference to your letter No. 15287/28 of August 10th I am directed by Lord Cushendun to return to you herewith as requested, the extract from the "East African Standard" of June 16th relating to the reduction of the sentence of imprisonment passed on Fritz Klopfer in Kenya Colony.

referred to
15064/28 K

I am to add that the original enclosure to the Colonial Office letter No. 15287/28 of July 9th last was returned to you under cover of the Foreign Office letter of July 26th last.

I am,

Sir,

Your obedient servant,

C. Horan Smith

The Under Secretary of State,
Colonial Office.

I am to request that
the enclosure to this letter may
be returned to this Dept. in due
course of regular mail. The copy to
the letter from this Dept. of the

Yours truly,
A. C.

In any further communication on this subject, please quote

No. *C 5341/5041/18*

and address - not to any person by name, but to -

"The Under-Secretary of State," Foreign Office, London, S.W.1.

3
2

THE Under-Secretary of State for Foreign Affairs presents his compliments to *the Under Secretary of State for the Colonies* and, by direction of the Secretary of State, transmits herewith copy of the under-mentioned paper.

Foreign Office

27 JUL 1928

July 25, 1928

Reference to previous correspondence:

Colonial Office 15287/28

July 9 (No. 2)

Description of Enclosure.

Name and Date

Subject

*German Ambassador
July 25
and original enclosure
to letter under reference*

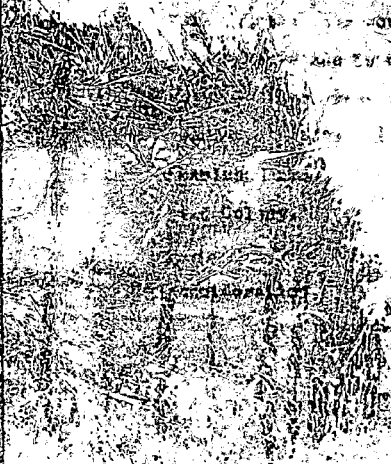
*Sentence of imprisonment
passed on Fritz Klopfer
in East Africa*

*(Re-attached to 15064/28
No. 14)*

Similar letter sent to

*30 AUG 1928
AUG 1928
Chas. Spence, 26 Feb. 6.25*

"before any Judge or other Magistrate within the Colony,
 "a pardon, either free or subject to lawful conditions,
 "or any remission of the sentence passed on such offender,
 "or any respite of the execution of such sentence for such
 "time as the Governor thinks fit, and may remit any fines,
 "or any duties, or any taxes due or accrued to Us. Provided
 "that the Governor shall in no case, except where



...of a political nature unaccompanied
 ...of any
 ...that the offender shall be
 ...himself or be otherwise

...at least

...obedient servant,

...of state

...O. HOWARD SMITH.

Edinburgh 2nd 9th May 1928

I am to advise the

of Kenya
the improvement of grant

has been made in certain cases
under the following clause

of the Letters Patent dated 12th

(constituting his title)

11th Sept 1920 :-

XIX When any crime is

committed
the said

whereof described
(constituting his title)

I am to request that the
enclosure of this letter may be
retained to the effect
I am etc

[Extra to
made in
copy of
1927
concluded
in 10 days]

(Signed) A. G. C. PARKINSON

In any further communication on this subject, please specify

No. C 5041/5041/18

25

and address

Foreign Office

not to any person or office

but to

S.W.1.

The Under-Secretary of State for Foreign Affairs, London, S.W.1.

2nd July, 1928.

Sir,

I am directed by Secretary Sir Austen Chamberlain to state that Prince Bismarck, Secretary at the Legation to this Court, visited the Foreign Office and made a statement as follows:

... was accompanied by telegraphist ... former called ... of 1915 ... our 1919

... because that ... because that ...

... because the Judge of Appeal was unable to make further research into the facts, but had to accept them as they appeared in the evidence given before the Lower Court.

Furthermore, the Consul alleges that public opinion, as exemplified by the "East African Standard", considers that the punishment is too heavy.

The Consul himself went to Nairobi with the defendant and asked the Chief Secretary for a retrial as a measure of grace. The whole question will probably be dealt with by the Governor in Council, who will decide what measure of grace, if any.

The Under-Secretary of State for Foreign Affairs, Colonial Office.

Handwritten notes:
C 5041/5041/18
25
2nd July 1928

any, can be accorded. Meanwhile Herr Klöpfer is in prison.

The German Ministry for Foreign Affairs has not asked for any intervention on the part of His Majesty's Government, but merely that their attention should be drawn informally to the facts as stated, and that if and when any further developments in this case become known in London they may be brought to the notice of the Embassy, which will in turn report them to the Wilhelmstrasse.

2. I am to enquire whether the Secretary of State for the Colonies is in possession of any information on this subject which can suitably or usefully be transmitted to the German Embassy.

I am,

Sir,

Your obedient Servant,

James Sargent

1928

Kenya

No. *15290*

SUBJECT

CO 533/379

Kenya Uganda Railway

Trunk - Mt. Kenya Extension

Final Report

Previous

x 10223/18 Kenya

Subsequent

*Lu 18518/29 (Amendment)
of route*

FILE C



Mich. Jan. 1898 SR and by the will of
result]

W.M.L.
11/17/28

I have compared the report
received from
at the least that says
it was done
very well done
same

Accd
B. J.
to
to

1320028 in Aug) 31 AUG 1928
Nov 14 2

I have not returned the
reports as requested in No 3.

now remain. pl.
11/10/28

Give them another fortnight
Glasgow
B. J.

? received now
afford
There is no hurry
27. 12. 28
Give them till 1.12.28
G. J. 28/10

Return the Final Reports and encloses a copy
of the comments thereof of Messrs. Rendel
Palmer and Tritton to whom the documents were
referred.

I think it is better to
be done by
and (in a paper)
of
Glasgow
W.M.L.
11/11/28
Accd
11/11/28
to

To Mr. Y 11/11/28 (1/2 & 4). A/11/19/28

4 4



ALL COMMUNICATIONS
TO BE ADDRESSED TO THE
CROWN AGENTS FOR THE COLONIES,
THE FOLLOWING REFERENCE AND THE
DATE OF THIS LETTER BEING QUOTED.

4, MILLBANK,
WESTMINSTER,
LONDON, S.W.1.

E. 283/34

TELEGRAMS: CROWN LONDON
TELEPHONE: 5754 VICTORIA

9th November 1928

Sir,

In accordance with your request I have the honour to return herewith the copies of the final reports of the Resident Engineer on the construction of the Extension of the Kenya and Uganda Railway from Turbo to Mbulamuti which accompanied your letter No. 15290 dated 31st August.

2

*No. 3 A/c
17 Copy to H.C.T. 19/10/28
1/11/28*

For the information of the Secretary of State I also enclose a copy of the comments thereon of Messrs Rendel, Palmer and Tritton, to whom we referred the documents for perusal.

22.10.28

I have the honour to be,

Sir,

Your obedient servant.

for CROWN AGENTS.

The Under Secretary of State,
COLONIAL OFFICE.

S. W. 1.

CA

5. It is not shown in the report what rates were charged on the parent line for the carriage of construction materials. In the case of the construction of the Usin Gishu Railway, the sum of £125,000 was charged by the Uganda Railway for carriage of railway materials, representing a capital charge of £850 per mile constructed. The Uganda Extension being longer and the load greater it would be that the cost of the material might be materially increased by the charges on the parent line for rail

6. The greater part of the saving in cost is due to the saving in length of construction in works arising from the adoption of trestle grades and the fact that where difficulties experienced in working them

total estimate of 1924.

and Appendix are

Yours faithfully,

General Manager of the Railway.

(Sd/-) J. P. GARDNER



7/1

HIGH COMMISSIONER
FOR TRANSPORT

OFFICE OF THE HIGH COMMISSIONER FOR TRANSPORT,
GOVERNMENT HOUSE,
NAIROBI,
KENYA.

No. 80

RECEIVED
25 JUN 1928
OFFICE

6th June 1928.

Sir,

I have the honour to transmit for your information a copy of the final report of the Resident Engineer on the construction of the Railway extension from Turbo to Mbulamuti, together with covering letters by the Chief Engineer and the General Manager.

2. The country through which this construction passes proved to be most unhealthy for staff living under the somewhat adverse conditions which are inevitable in the work of railway construction and I desire to record my sincere appreciation of the valuable services of the Resident Engineer and his staff.

3. The question of momentum grass to which reference is made on page 13 of the report is receiving close consideration and will form the subject of separate correspondence at a later date.

I have the honour to be,

Sir,

Your most obedient,

humble servant,

Edward Glegg
HIGH COMMISSIONER

THE RIGHT HONORABLE
LIEUTENANT GOVERNOR
L. C. S. ALGERY, M.P.,
SECRETARY OF STATE FOR THE
COLONIES,
LONDON, S.W.1.

*Ans d. 86 -
copy to d. (Vol. 2. no. 3) 31 AUG 1928*

COPY

KENYA AND UGANDA RAILWAYS
AND HARBOURS

General Manager's Office,
NAIROBI.

Reference F. 5123 Vol. II

19th May 1955

The Hon. Commissioner of Transport

UGANDA EXTENSION - SPECIAL REPORT

I forward herewith, for the Hon. Commissioner's information and transmission to the Secretary of State, a copy of the final Report of the Resident Engineer of the Uganda Extension, together with a copy of the Chief Engineer's covering letter.

2. So far as the construction of the line is concerned, I associate myself entirely with General Rhodes' remarks in regard to Mr. H.G. Dainger and his staff. The Uganda Extension is a monument to the ability, perseverance and pluck of the engineer who carried out the actual construction and his staff, a number of whom lost their lives during the period of construction, while the health of many others was seriously impaired as a result of the conditions under which they had to work in what turned out to be one of the most unhealthy parts of the country.

3. With regard to the question of momentum grades (to which reference is made) I have already previously registered my disagreement with the use of the grades adopted. This matter is still under consideration.

(Signed) G.B. [Signature]

General Manager

section of line passes through a class of country somewhat heavier than the Eldoret-Tamb section of the Uasin-Gishu Railway, where a 1.5% ruling grade already exists.

8. In the meantime the whole line is being worked to speed restriction of 25 m.p.h. with a 1.5% leading. The expenditure recommended by the acting Chief Engineer in his No. 10/S dated 14.4.28 to bring about this result is entirely unnecessary.

9. With reference to Page 34, it can be seen that the cost per mile in Uganda is slightly higher than in Kenya, in spite of the fact that over the whole the work is considerably lighter in Uganda.

10. The principal reasons for this are as follows:-

(a) High Cost of Labour. An expansive organisation was introduced to recruit labour. The labour itself was unaccustomed to the work and it was frequently difficult to get a reasonable amount of work out of each boy. To obtain the best results it is essential that the organisation created for obtaining the labour and watching over their welfare should also be equally anxious to see that in return the labour gives a full day's work. The difficulty in finding suitable supervision as explained by the Resident Engineer, also contributed largely to the high cost of work done.

(b) High Cost of Transport. Under the circumstances this cost was unavoidable and contributed largely to the extra cost of materials and increased the cost of handling.

(c) High Cost of Medical arrangements and Carriage. In a country such as that through which the work was carried out the cost of these items is bound to be high.

11. General clauses have also been higher than anticipated both in Kenya and Uganda. This is due to sickness, and unforeseen delays largely due to the failure of some of the contractors to complete their contracts in the contract time.

12. The country through which the railway is being built is a most unhealthy one under construction conditions and great credit is due to the Resident Engineer and his staff for the way they stuck to their posts. The list of those individuals who gave their lives to this work (page 34) bears eloquent testimony in this respect.

13. For a considerable period the two ends of the construction were 100 miles apart and communication had to be maintained by an indifferent telegraph service, bicycle couriers, and by motor car van trials that in the wet weather could only be called roads by courtesy.

14. Under all these circumstances the Resident Engineer Mr H.O. Dempster and his staff deserve great credit for completing the line in such a reasonable time and at a very reasonable cost. I wish to record my personal appreciation of their keenness and zeal and devotion to duty under what were at times very unpleasant circumstances.

(Signed) C.D. ROOPE,
Chief Engineer,
Kenya & Uganda Railways & Harbours.

KENYA AND UGANDA RAILWAY - UGANDA EXTENSION

COMPLETION REPORT.

History. As early as 1891-2 a reconnaissance survey was made by Captain Macdonald of a railway route which passed through the Usain Gishu District, Northern Kavirondo, to Uganda.

The route from Nakuru to Kisumu was however adopted and the line built from Mombasa to the latter point and no further steps appear to have been taken with regard to the building of a railway connecting the sea port to the Nile until 1914 when a preliminary survey was made as far as Kamies via the Usain Gishu Plateau under Mr. Church, the Chief Engineer, Uganda Railway. The construction of the Usain Gishu Railway from Nakuru to Turbo was commenced in 1922 and completed early in 1925.

An Honorary Committee was appointed by the Government of Uganda at the end of 1923 to consider the most suitable route for a railway extension to connect with the Busoga Railway and it was decided that the line should follow as direct a route as possible from Tororo to Mbulamuti.

The preliminary surveys and estimates of a line from Turbo to Mbulamuti were completed in August 1924 and the line was then estimated to be 194 miles long and to cost £1,75,821 or £8,111 per mile.

HISTORY.
CONT.

In August 1924 Mr. H. G. Dempster, Assoc. M. Inst. C.E. was appointed Resident Engineer, Uganda Extension, and preparations were made for constructing the line.

Immediately prior and subsequent to the commencement of construction the whole line was examined and realigned in detail with the result that the line was reduced in length by about 10 miles to a total of 163 1/2 miles and in cost from over one and a half million to £1,201,029, which is equivalent to £6536 per mile.

The building of staff quarters, stores, etc. was commenced at Turbo in October, 1924 but owing to the delays in the completion of the Uasin Gishu Railway, construction of the Line did not actually commence until January 1926.

The line was opened for construction traffic between Turbo and Tororo, and also between Abulamuti and Masinzi on February 1st 1927 and the section of 30 miles between Turbo and Broderick Falls was handed over to open lines on July 1st 1927. The whole line was opened for public traffic on the 9th of January 1928 just three years after construction was commenced.

GENERAL.

For a distance of 18 miles after leaving Turbo the line passes through farms occupied by white settlers and there are two stations, Kipkarren River and Lugari, on this section which will serve a prosperous and progressive community.

GENERAL. climatic conditions and also to a certain extent, by the exceptional wet season which was experienced in 1926.

The earthworks, bridges and culverts on the 30 mile section from ... were heavy, there being a series of solid rock bridges of considerable length and many arched culverts.

The line crosses the drainage of the country and several ... in Mount ... have to be crossed.

... do not call for any special ... in regard to the method of ... which were ... in ...

... special trucks ... the rails ... and were then ... together in ... pushed forward by a locomotive ... the first span ... and the counterweights by those ... it ... first pier ... even ... the ... of the river ... behind ... In the ... except these over ... were successfully launched in this manner, and caused only very short delays to the progress of platelaying, while avoiding the necessity of all temporary staging.

GENERAL
CONT.

The Mpologoma bridge and causeway was the most interesting problem of construction and when operations commenced early in 1925 it was seen that the crossing of the Mpologoma Swamp would be a difficult and tedious job. A commencement was therefore made as soon as possible by cutting a track through the forest and building a road to the site so that material, plant & food supplies could be brought from Mjani Port, a distance of 70 miles by motor lorries.

The so called swamp is a little over a mile wide and the water which has a uniform depth of 10 feet to 12 feet is covered with a dense mass of papyrus. The bed of the swamp consists of a layer of black vegetable mud lying on clay.

A considerable amount of investigation had to be undertaken before plans were complete, but it was eventually decided to build a murrum causeway leaving an opening spanned by five 60 feet girder bridges resting on concrete cylinders sunk to rock. The rock proved to be forty five feet to fifty five feet below water level, and although minor difficulties and unexpected problems were encountered the work progressed satisfactorily and the substructure of the bridge was completed by the time the rails reached the site.

LABOUR
AND
CONTRACTS

Practically all works in Kenya with the exception of buildings were done by contractors while in Uganda all works west of Tororo were done by departmental labour.

LABOUR
AND
CONTRACTS
QUEST.

at rates based on those obtaining in the first contract but with due regard to the local conditions which varied in some details.

These contracts proved to be most unsatisfactory and were not complete without numerous disputes and compromises.

The main cause of these troubles was the fact that the contractors had undertaken to excavate Class 1 hard cubic rock at the rate of 100 cubic feet per day and cubic feet were payable at the rate of 10/- per cubic foot.

The contractors, however, found it necessary to pay the contractors a premium to excavate the hard rock and they were paid which was not based on the rate of 10/- per cubic foot in order that they would be compensated for their losses on excavating material which was actually hard rock.

The practice of awarding contracts to tenderers, that is to say in which prices are not prearranged, has been found to be a very bad one and it is believed that the only way to avoid the serious conflicts and result in serious loss to the Administration.

CC 518 379

LABOUR
AND
CONTRACTS
CONT.

As however there was no practical alternative the system was continued and the work completed by departmental labour.

An exception to this system was made in the case of a few heavy banks and large culverts in the vicinity of Tororo which were required to be completed in a short time and were let out as small contracts to European and Indian contractors in the same way as was done on the section between Braderick Falls and the Uganda Border.

On the section west of Mpologoma natives recruited in the West Nile Province were brought to the work by the Labour Department of the Uganda Protectorate.

The costs of recruiting were high as the necessary organisation was extensive but the wages were low and after the first few months these natives worked well and on the whole appeared to be well satisfied with the conditions many signing on for several successive contracts of 4 months each and others returning to the work after going back to their homes after completing a period of service.

The wages paid to native unskilled labourers varied very considerably both in different districts and at different periods of the three years of construction.

LABOUR future all native labourers must be housed in wood
AND
CONTRACTS and iron portable huts.
GOVT.

The Uganda Government is to be congratulated on the excellent medical service and system of enforcing the regulations regarding housing and feeding of native labourers on railway construction which although they are sometimes tedious and perhaps annoying to engineers whose duty it is to push forward the work with speed and economy, yet are effective and consistent with the policy of raising the standard of living and teaching the native how to work and progress.

SURVEYS.

Three survey parties were engaged on the preliminary tachometer survey between February and August 1924 and 143 miles of through line were surveyed at a cost of £40 per mile of selected main line including the cost of trial lines which were abandoned.

This does not include the field work and preparation of plans of the section between the Esola and Sir... a length of 28 miles which was surveyed in 1914-15 by Col. J.K. Robertson and these colour plans were used for the preparation of the estimates of the line from Turbo to Malaba River the border of Kenya and Uganda.

The working out of the line was commenced in August 1924 one party starting from Turbo, another from Geraró going west and a third from Mbulamuti working east.

SURVEYS
COST.

These three parties were continuously employed up to May 1925 but subsequently many alternative lines and minor improvements were surveyed and staked out either by one of the regular survey parties which was employed after the above date or by members of the construction staff. A length of 12 miles was surveyed and staked out between Nagongera and Kpologoma by the construction staff engaged on the Kpologoma causeway when a better route was found and adopted.

The total cost of staking out including the survey of alternative routes and improvements was £9663/- or £52/- per mile of completed main line.

A ruling grade of 1- was adopted, compensated at the rate of 0.03% per degree of curvature, and 10 degrees curves were the sharpest allowed.

All curves 4° and sharper had transition curves.

An analysis of curvature is given on page 1 of the appendix to this report and for this purpose the line is divided into three sections and also treated as a whole.

The first section from Turbo to Broderick Falls is an entirely different class of country to that beyond and for this reason is dealt with separately in the Curve Analysis. The curvature on the first mentioned section averages 185 degrees per mile while that on the remainder averages 59 degrees per mile.

6

SURVEYS:
CFT.

The original survey from Turbo to Mbulamuti did not provide for momentum grades while the adopted survey did and the comparative figures relative to earthwork, degrees of curvature and length are given in the table below.

1 st line without Momentum Grades	Cub. feet Earthworks	Degree of Curvature	Len Miles
	5,700,300	16,802	194
2 nd line with Momentum	5,000,000	13,320	184

with momentum grades a great saving in earthworks and bridges and culverts can usually be obtained in an undulating country where many valleys have to be crossed.

LAND

Very little compensation had to be paid to owners for land expropriated for railway purposes but in Uganda the payments fixed by the local Authorities for crops, especially bananas, were high and £1840 was paid under this head being 20% in excess of the estimate.

FORMATICS
EARTHWORKS

The following is a list of the principal items in the schedule under earthworks for the Turbo-Broadrick-Mbulamuti section.

Class I per 1000 cu. feet	Shs. 125/- to 160/-
" III " " "	Shs. 25/- to 30/-

CLASS II All material intermediate between classes I and III, per 1000 cub. feet Shs. 40/- min. to Shs. 55/-.

As already stated the prices for Class I were lower than actual cost and the schedule prices were not adhered to.

ESTIMATE
OF
WORKS
AND
CONTRACTS

the price for the other classes were on the high side but this did not prevent the contractors pressing their claims for favourable consideration as regards class I.

Between Eroderik Hills and Tororo the earthworks were let to small contractors and the schedule rates were as follows:-

Class I	Shs. 100/-	per 1000 cu. feet
Class II	Shs. 65/-	" " "
Class III	Shs. 30/-	" " "

Although practically no Class I material was set with on this section the less experienced contractors required a lot of supervision and attention, this system worked well and the work was done cheaply.

In Uganda all earthworks with the exception of some large banks near Tororo were done by departmental labour supervised by earthworks inspectors and Nubian headmen.

Between the Mpologoma and Bulamuti a length of 65 miles the earthworks were commenced on 1st January 1925 and completed in October, 1926.

All labour employed during the first five months was recruited from East Hill and in May 1925 the total number employed reached the maximum of 4000.

FORMATION
EARTHWORK CONT.

of the Hukian headmen with the result that the records cannot be relied on to give accurate figures. The actual amount of cubic feet excavated per labourer is

There is a serious and almost insurmountable difficulty in employing large numbers of labourers without letting at least a large percentage of the work out to contractors. Adequate supervision is practically unobtainable. Yet whitemen will continue to do their utmost if they are employed by the "Government" at a monthly wage, and there is a class of whitemen which has been accustomed to undertake and supervise contracts and which prefers to go elsewhere rather than to direct employment. There are many Indians who make fairly satisfactory contractors but quite unsuitable for earthwork. Inexperience or supervisors of unskilled labour.

Some whitemen are capable of supervising four or five hundred labourers, distributed over several miles of line and getting even a reasonable amount of work out of them but under no circumstances existing in Uganda earthwork contractors had to accept this as the only alternative, and the results as a whole were only as good as could be expected.

The most important work of the engineer was to prevent unnecessary work being done and to see that uneconomical methods were not adopted in consequence of the inexperience of both labourers and supervisors.

30

**FORMATION
EARTHWORKS CONT.**

It is however doubtful if the total cost of earthworks would under the circumstances have been less if more supervisors had been available as their wages would have probably more than outweighed the consequent increased output of the labourer.

If continuous and steady railway construction in Uganda could be looked forward to, a staff of permanent supervisors should be imported and trained on a system similar to that adopted in Nigeria and the Sudan.

The total cost of earthworks including clearing and drains in Uganda was £.115,800 or £.1,092 per mile which is £.16 per mile less than the estimate.

The clearing of bush and trees was light throughout except in Uganda where occasional patches of forest was met with and a few large mull trees had to be felled and cleared.

The earthworks at the Mpologoma swamp call for special mention as although the case is perhaps unique and similar conditions are not likely to be met with again the following are of interest:

Length of Causeway ...	6000 ft.
Average depth of water ...	10 ft.
Average height of formation above W.L. ...	7 ft.

FORMATION
EARTHWORKS CONT.

After the permanent girders of the bridge had been placed the bank under them was removed, the temporary opening closed and the bank on each side of the bridge raised to the final level. In December 1927 just three years after operations had commenced.

BRIDGES AND
CULVERTS.

An abstract of bridges and culverts is given on page 2 of the appendix together with plans of all bridges and culverts. The types of culverts were designed with a view to avoiding as much as possible the cutting of timber shuttering.

The practice which obtains on the Kenya and Uganda Railway of supplying contractors with timber for shuttering is one that cannot be recommended and should be discontinued as soon as the custom can be broken away from. It leads to a great waste of timber as although contractors are supposed to pay for losses beyond fair wear and tear and waste due to cutting, it is quite impossible to define what is fair. Contractors should supply their own shuttering.

The building of the culverts and bridges on the first 38 miles from Turbo to Broderick Falls was part of the three large contracts for earthworks and bridges which as has already been stated, were let early in 1925.

An analysis of the costs of the culverts and bridges on Contract No. 7 and also of those on the section beyond Broderick Falls are given in appendix page 3 and it will be seen that the unit

BRIDGES AND
CULVERTS CONT.

costs of the different works on Contract 17 only vary slightly.

This variation is only due to the different distances that cement and shuttering had to be transported and it must be remembered that these detailed records are of little value in determining the actual cost of the work as although it is known that owing to varying local conditions of weather, floods, foundations and difficulties of transport the profit made by the contractor on each piece of work varies within very wide margins his actual profit (or loss) is not known.

It is for this reason that costs of each job are of little or no value for estimating future works unless separate costs of excavations, pumping, timbering, transport, shuttering are accurately kept.

Any one of these items may cost either a large percentage or a negligible percentage of the total cost and it is not sufficient to append a note to the effect that excavations or pumping was expensive or otherwise.

A multitude of expenditure heads may be kept on a concentrated job but it is impracticable to do so on works spread over 100 miles of country with illiterate foremen and headmen to keep the records.

BRIDGES AND
CULVERTS
CONT.

Between the border of Kenya and Uganda and a point a few miles beyond Tororo there are five culverts of 8 ft. or 12 ft. arches but beyond that there are no built culverts for the whole distance of 90 miles from Mile 241 to mile 331.

The Nyolagoma is bridged by five 60 ft. spans and is described below while waterways consisting of batteries of 3 ft. diameter pipes are provided at the Ngombwa and other similar swamps.

A triangular area has been cleared of papyrus and reeds at the inlet of these batteries of pipes and it is important that these clearances be maintained as otherwise the pipes will become blocked and the water rise over the bank.

All other openings consist of single, double or treble 3 ft. or 2 ft. diameter pipes.

A drawing (No. 57) of the battery of pipes at the Ngombwa and Lumbuye swamps shown in the appendix is attached to this report. The concrete slab or raft on which the pipes rest is supported on the soft clay or mud of the swamps and no solid foundation is required such as would have been necessary if bridge or culvert openings had been adopted.

Nyolagoma Bridge. This consists of five deck spans of 60 ft. each resting on six pairs of concrete cylinders sunk to rock at a depth of from 45 ft. to 55 ft. below water level.

BRIDGES AND
CONVERTERS
CONT.

A bank was first built on the site of the bridge and levelled up at a height of about 2 ft. above water level and on this bank the work of building and sinking the cylinders was carried out.

A temporary opening 80 ft. wide was left in the causeway some 500 ft. from the site of the bridge through which the water flowed during the construction of the bridge.

The details of the substructure are shown on the drawing No. 55 in the appendix. Cylinders were sunk without difficulty for the first 20 ft. to 25 ft. by digging in the dry and removing the material excavated in skips and hand derrick cranes. The papyrus mud proved to be quite water tight and no difficulty was experienced in keeping the cylinders dry by baling.

Compressed air in perforated pipes driven down by hand close to the outside of the cylinders relieved the skin friction and there was no difficulty in sinking them to the above mentioned depth.

Below this, beds of sand were met with and water entered freely, necessitating the use of pulsometer pumps. Sinking was continued in this manner down to as much as 34 feet in some cases but eventually excavations in all cylinders had to be done in the wet by Frisman grabs worked by steam cranes.

BRIDGES AND
CULVERTS

Below a level of 30-35 feet the river bed had to be lowered with sand and gravel. The width and the spacing between the piles was increased a distance of 200 feet from the one bank where rock was obtained.

The cost of these bridges not including superstructure was A. 10,250.

The five 50 ft. truck spans which comprise the superstructure of this bridge were all riveted up completely with rough flooring in the light workshops at Kampala. Sixty six trucks were used.

A temporary earth bank was built up on the left bank of the river and the water gauge tank with sluice immediately raised when reached the bridge.

The girder spans were lowered in position by a locomotive, the spans were lowered and the trucks withdrawn after which they were lowered on to the river.

All spans in place and the rails reached the river.

The earth bank was then removed and the river allowed to flow under the bridge while at the same time the temporary opening in the Causeway was closed.

The approach bank on either side of the bridge was then raised approximately 7 ft. to the permanent formation level.

39

BRIDGES AND
CULVERTS
WORK.

The superstructures of all the 60 ft. spans in Sanya were launched by the method shown in Drawing No. 47A.

The spans were in all cases riveted up on site at Sanyo and prepared for launching in trucks and were run out to the site on girder trucks and lowered on to rollers. They were then connected together and pushed in the manner shown and pushed forward by locomotives into the positions over the piers. They were subsequently disconnected and lowered on to the girder beds.

The following bridges were launched in a similar manner to that described and shown, improvements in detail being introduced in the later bridges as experience was gained on the earlier ones.

Hzoia Bridge	2 spans of 60 ft. each erected in 10 days
Myanga Bridge	2 spans of 60 ft. each erected in 4 days
Malikisi Bridge	2 spans of 60 ft. each erected in 6 days
Malaba Bridge	3 spans of 60 ft. each erected in 6 days.

All the above bridges were erected without any accidents or mishaps and the times given are from the arrival of the first span on the site to the placing of the last span on its girder beds. Heavy rains delayed progress on the Hzoia and Malikisi Bridges.

FENCING
ETC.

The type of Gradient Post adopted is shown on page 4 of this appendix. It is an improvement on the conventional type as the post can always be seen (night or day) by the driver in whichever direction the train is travelling. Its position also indicates to the platelayer the ends of the vertical curves.

The line has been chained throughout after completion and the mileage of all culverts, pipes, level crossings, points and crossings, etc recorded in a "Line Book" accompanying this report.

ELECTRIC
TELEGRAPHS

The work of erecting and installing was carried out by the Kenya and Uganda Postal and Telegraph Department. Wooden Poles were used throughout carrying the wires.

Tyres Tablets were installed at all stations.

BALLAST AND
PERMANENT WAY

The rails throughout were 50 lb. B.S. Section and steel sleepers with reversible keys were used. Rail joints were staggered on curves with 12 sleepers per 30 ft. rail on straight and 13 sleepers on curves as shown on page 5 of the Appendix. Except in some wet cuttings between Turbo and Braderick Falls where broken stone ballast was used, murrum or red earth ballast was used throughout. A clearance of 1,400 feet is provided at all stations and crossing loops.

BALLAST AND PERMANENT WAY
CONT.

41

The platelaying and ballasting between Turbo and Tororo, a distance of approximately 85 miles, was let to a contractor and his failure to complete the work within the specified time or within a reasonable period caused the completion of the line and hence the start of the road to be delayed for some time.

The contractor was not held responsible for any delay in the work due to the fact that the power and number of trucks used by the contractor which was very excessive.

The contractor's price for ballast and tracks for the contractor's use of all ballast is not the amount set by Government as there is no Government price for ballast.

Between Tororo and Musomoni platelaying and ballasting was done experimentally with the supervision of the platelaying officer Tororo and Musomoni. Work was done in 1944, the contractor only doing the offloading and laying of the rails and sleepers. The results were very satisfactory and the work was done well and expeditiously.

The platelaying and ballasting in the contract and in Uganda by small contractor and departmental labour is given at the end of this report.

STATIONS AND BUILDINGS

All buildings except goods sheds were of concrete block. The quarters for Europeans

STATIONS AND
BUILDINGS
(CONT.)

and Asiatics having tiled roofs, other buildings being roofed with corrugated iron.

42

All buildings were built to standard type designs approved by the Chief Engineer with the exception of those shown on pages 5 to 8 of the appendix in which the type was departed from in detail.

All buildings with the exception of those noted above were built by departmental labour under the supervision of the Inspector of Works.

The following buildings at Tabora were built by contract the department supplying material only:

<u>No. of Buildings.</u>	<u>Type of building</u>	<u>Prog. No.</u>	<u>Contractor.</u>
5	11 unit native landings	2012	Simons
1	10 " " "	Rev. 25.	Graham
1	6 " " "	do.	Graham
1	1 (2) Unit Superior grade African qtr.	2088	Simons
1	12-3 Unit Asiatic quarters	478 rev.	"
1	1 Unit African washing room.	2234	"
1	Electric Offices	2887	"
1	4 roomed European House-Junior Officer	7704	"
1	3 roomed European House-Junior Officer	2698	"
1	3 roomed European house-Senior Officer	2716	"

It was necessary to let these by contract as they could not be economically commenced before

STATIONS AND BUILDINGS
CONT.

the rails reached Tororo and then had to be pushed forward at a greater rate than could be undertaken by the Works Inspector with the labour and facilities at his disposal. The cost of the buildings built by contract was in most cases greatly in excess of those built by the department on the Kenya Division but the time element justified the additional expenditure. Departmental work was much more satisfactory and better work was obtained.

43

The cost of buildings ^{done} by departmental labour in the Mpologoma - Mbulamiti section was very high. This is mainly due to bad supervision but is partly accounted for by the high cost of transport and distance from source of supplies.

A list of the cost of all buildings is given at the end of this report each cost being recorded separately and in detail.

WATER STATIONS

The following is a description of the water supplies.

Lugari To Turbo 18 miles - rise 731 ft. To Broderick Falls 12 miles, fall 258 ft.

One No. 7 Blake's Hydrant pumping from Kipkarron River a distance of 3,000 ft. to a height of 83 ft.

Broderick Falls To Lugari 12 miles - rise 258 ft. To Bungoma 18 1/2 miles - fall 239 ft. Two No. 7 Blake's Hydrants pumping from Uzoia River to 30,000 gal. reservoir a distance of 3,000 ft. and height of 230 ft. From reservoir by gravity to Station. Distance 10,100 ft. fall 83 ft.

WATER STATIONS
CONC.

of 1,700 ft. The total rise to summit is 63 ft. but the rise for the section tank is only

The well at first provided, failed or hourly failed during the 1927-28 drought and a new well is now sunk.

Ab-lamati To Kairo 36 miles rise 42 ft. Water is pumped from the Nile a distance of 7,200 ft. and a height of 120 ft.

Sewage System For home sewage with septic tanks has been installed at the above stations except Tezzen and Kairo which are in hand at the

PLANT

A Compressor plant has been used on work exp. 1927-28 at the Epologone and the rest has been done

Unless further construction work is undertaken there use can be made of the plant it is available for the Uganda Extension to obtain available for transfer and the expenditure under this head will be high.

The total expenditure under this head is high at the present as shown in the expenditure statements attached.

ON WORK

Expenditure under this head is excessive chiefly owing to the time the work has taken to complete in consequence of delays by contractors.

46

GENERAL
CHARGES
CONT.

A very large staff has been employed, the total number of Europeans who have received appointments on the work being 222 and the corresponding number of Asiatics being 134.

I regret to record that 10 Europeans and 1 Asiatic died either on the work or immediately after leaving it. A list of their names is given below:-

Major G. Lawrence Brown,
Assistant Engineer, died 30/3/1927.

Heron H. Pagarakiola,
Surveyor, died 22/2/1926

E. G. Gormack, Assistant Engineer
died Sept. 1927

R. Eland, Clerk died 1/7/1925

W. J. B. Edwards, Asst. Line Controller
died 26/11/1927

J. W. Edwards, Work Foreman.
died 25/1/1927

A. B. O'Donnell, Earthworks Inspector.
died 24/7/1926.

D. Jackson, Asst. Platelayer,
died 10/7/1927

T. Rinkering, Earthworks Inspector.
died 27/6/1927.

W. H. P. Parker, Earthworks Inspector.
died March, 1926.

Wazir Khan, S.P.V.I. Died 27/9/1926.

An expenditure statement which has been brought up-to-date as far as possible is attached and it will be noted that the total cost of the Kenya Division is £522,147 or £6720 per mile and of the Uganda Division £115,344 or £6745 per mile while the combined cost is £1,237,490 or £6732 per mile. These figures are subject to modification when all debits and credits have been passed.

APPROXIMATE STATEMENT OF EXPENDITURE TO END OF MARCH 1928
 SHOWING PERCENTAGE IN COMPARISON WITH ESTIMATE 915/11. DOAN
 OF £2,500,000 (108.75 MILES)

MAIN HEAD.	ESTIMATED AMOUNT		EXPENDITURE TO END OF MARCH 1928.		PERCENTAGE OF EXPENDITURE IN COMPARISON WITH ESTIMATED AMOUNT	RATE PER MILE.
	£.	SHS. CTS.	SHS.	CTS.		
Preliminary Expenditure	£ 208,000.	00	215,734.	51		
I. Land	4,230.	00	37,404.	74	104%	2040/-
II. Formation	5,549,491.	00	2,314,850.	74	88%	354/-
III. Bridges etc.	1,009,770.	00	817,623.	22	99%	21,890/-
IV. Working	22,822.	00	9,181.	66	80%	17,732/-
V. Station Telegraphs	237,200.	00	187,843.	65	40%	87/-
VI. Permanent Way and Ballast	5,800,537.	00	5,141,277.	81	79%	1,776/-
VII. Stations and Buildings	2,025,600.	00	1,850,334.	42	89%	48,617/-
IX. Plant	99,600.	00	420,147.	80	91%	17,497/-
X. Rolling Stock	30,000.	00	11,959.	05	42%	3,073/-
XI. General Charges	2,705,572.	00	3,250,533.	84	40%	113/-
Total	14,552,322.	00	14,248,861.	44	120%	30,632/-
			2,715,344		98%	154,911/-
						56,745

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LENGTH OF TUNNEL 194.27 MILES			
GRADIENT	NUMBER OF ARCH.	LENGTHS IN MILES	PERCENTAGE OF TOTAL LENGTH OF TUNNEL
0.42	1	0.26	.15
0.43	2	0.46	.24
0.44	1	0.47	.25
0.45	2	0.45	.24
0.46	2	0.88	.37
0.462	1	0.23	.12
0.47	1	0.27	.15
0.48	1	0.23	.12
0.50	18	5.30	2.89
0.515	3	0.36	.20
0.518	1	0.22	.12
0.52	2	1.08	.59
0.545	1	0.13	.07
0.55	4	1.46	.81
0.59	1	0.44	.24
0.60	11	2.87	1.56
0.612	1	0.23	.13
0.614	1	0.14	.07
0.623	1	0.06	.03
0.65	3	1.07	.58
0.66	2	0.49	.26
0.68	1	0.19	.10
0.696	1	0.13	.07
0.70	1	5.97	3.10
0.715	1	0.17	.10
0.73	5	1.32	.73
0.75	3	0.81	.46
0.76	4	0.98	.54
0.77	1	0.22	.13
0.78	2	0.47	.25
0.785	1	0.27	.15

GRADIENT.	NUMBER OF EXCH.	LENGTH OF LINE 184.27 MILES	
		LENGTH IN MILES.	PERCENTAGE OF TOTAL LENGTH OF LINE.
0.80	19	4.84	2.64
0.809	1	0.12	.07
0.811	1	0.74	.40
0.82	3	0.66	.36
0.83	1	0.28	.16
0.84	3	0.68	.42
0.85	12	2.48	1.35
0.856	1	0.32	.20
0.870	1	0.31	.17
0.883	6	1.82	1.00
0.885	1	0.22	.12
0.89	1	0.31	.18
0.90	20	6.52	3.56
0.907	1	0.22	.12
0.91	2	0.78	.42
0.92	1	0.09	.05
0.93	1	0.65	.36
0.95	9	6.21	3.39
0.96	2	0.59	.32
0.97	6	1.13	.62
0.98	1	0.31	.17
1.00	58	18.10	9.89
1.01	1	0.20	.11
1.02	1	0.46	.25
1.03	1	0.25	.13
1.05	7	3.04	1.66
1.06	1	0.12	.06
1.09	1	0.36	.20
1.10	9	4.34	2.37
1.107	2	0.60	.33
1.14	1	0.28	.15
		27.3	14.87

55

GRADIENT	LENGTH OF LINE ABANDONED		
	NUMBER OF MACH.	LENGTH IN MILES	PERCENTAGE OF TOTAL LENGTH OF LINE
1.80	4	2.20	.75
1.80	6	1.70	.56
1.95	1	0.37	.12
2.00	19	5.28	1.73
2.05	1	0.12	.04
2.10	4	1.36	.44
2.15	1	0.19	.06
2.15	1	0.42	.14
2.20	4	1.09	.36
2.22	1	0.28	.09
2.26	1	0.43	.14
2.30	3	0.76	.25
2.40	1	0.18	.06
2.474	1	0.32	.11
2.60	10	2.72	.90
2.70	1	0.17	.06
TOTAL	622	193.15	100.00

The remainder of the line, i.e. 2.13 miles is laid along the widened formation of the Busoga line and conforms to the existing grade of that line.

STEEPEST GRADE..... 2.70 down
 LONGEST CONTINUOUS LENGTH
 OF STEEPEST GRADE..... 0.17 miles
 FOLLOWED BY..... LEVEL
 FOR..... 0.13 miles

Utah Extension
 Levee Camp
 134.27 Miles

BRIDGE ABSTRACT

FORM III

Class of Bridge.	Span.	Total No. of Spans.	Waterway Lin. Feet.
Slab Top Culvert	210'	2	4
	9'0"	1	9
Pipe Culvert	1'0"	1	1
	1'6"	1	1.65
	2'0"	184	376
	2'6"	24	60
	3'0"	130	360
Arch Bridges (Minor)	3'0"	2	5
	8'0"	63	519
	12'0"	27	324
	15'0"	5	75
Girder Bridges (Major)	40'0"	2	80
	60'0"	2	120
Waterway		Total 153	1114

Major Ditch
 Gen. " 820 Lin. Feet
 Minor " 693 "

Major Blvd.
 Gen. " 3.36 Lin. Feet
 Minor " 5.76 "

Total 1512 Lin. Feet.

Telegraph Pole Mileage	Chainage	Major Bridges	Arch Culverts and Flat Top Openings	Name of River if known and Bridge Number.
187/7 208/19	886945.75 333436.00	2-60' Girder Spans	3-6'x6'	Sio River 40
				Mnyanga River 41
215/14	361350.00		3-6'x6'	42
218/16 222/0	379750.00 397571.00		1-6'x4'	43
			1-6'x12'	KAKORRA R. 44
222/17	401685.00	2-60' Girder Spans		Malakini River 45
223/8	404700.00		2-6'x12'	Karjai River 46
224/18	412577.00	3-60' Girder Spans		Malaba River 47
227/3	560450.00		3-3'x6'	Nyamattunga River 48
227/9	961685.00		2-4'x6'	Nyalacotta River 49
230/1	975590.00		2-4x6'	Kanumiti River 50
251/19	985250.00		2-4'x6'	Kadiga River 51
358/6	2900.00		2-4'x12'	52
235/18	6200.00		2-4'x6'	53
238/11	16700.00		2-4'x6'	54
240/16	31850.00		1-4'x6'	55
267/18	124217.00	2-60' Girder Spans		Mpologoma River 56

STATIONS AND STATION SITES.

UGANDA RAILWAY 59
Metre Gauge
184.87 Miles

STATION	WATERING	ELEVATION CHANGING	STATION	STATIONS BETWEEN STATIONS										Kulu	Kulu	Kulu	Kulu	Kulu	Kulu	Kulu	Kulu	Kulu																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																		
				184.87	184.87	184.87	184.87	184.87	184.87	184.87	184.87	184.87	184.87										184.87	184.87	184.87	184.87	184.87	184.87	184.87	184.87	184.87																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
Srd			148	0	15	30	40	55	70	85	100	120	135	145	155	165	175	185	195	205	215	225	235	245	255	265	275	285	295	305	315	325	335	345	355	365	375	385	395	405	415	425	435	445	455	465	475	485	495	505	515	525	535	545	555	565	575	585	595	605	615	625	635	645	655	665	675	685	695	705	715	725	735	745	755	765	775	785	795	805	815	825	835	845	855	865	875	885	895	905	915	925	935	945	955	965	975	985	995	1005	1015	1025	1035	1045	1055	1065	1075	1085	1095	1105	1115	1125	1135	1145	1155	1165	1175	1185	1195	1205	1215	1225	1235	1245	1255	1265	1275	1285	1295	1305	1315	1325	1335	1345	1355	1365	1375	1385	1395	1405	1415	1425	1435	1445	1455	1465	1475	1485	1495	1505	1515	1525	1535	1545	1555	1565	1575	1585	1595	1605	1615	1625	1635	1645	1655	1665	1675	1685	1695	1705	1715	1725	1735	1745	1755	1765	1775	1785	1795	1805	1815	1825	1835	1845	1855	1865	1875	1885	1895	1905	1915	1925	1935	1945	1955	1965	1975	1985	1995	2005	2015	2025	2035	2045	2055	2065	2075	2085	2095	2105	2115	2125	2135	2145	2155	2165	2175	2185	2195	2205	2215	2225	2235	2245	2255	2265	2275	2285	2295	2305	2315	2325	2335	2345	2355	2365	2375	2385	2395	2405	2415	2425	2435	2445	2455	2465	2475	2485	2495	2505	2515	2525	2535	2545	2555	2565	2575	2585	2595	2605	2615	2625	2635	2645	2655	2665	2675	2685	2695	2705	2715	2725	2735	2745	2755	2765	2775	2785	2795	2805	2815	2825	2835	2845	2855	2865	2875	2885	2895	2905	2915	2925	2935	2945	2955	2965	2975	2985	2995	3005	3015	3025	3035	3045	3055	3065	3075	3085	3095	3105	3115	3125	3135	3145	3155	3165	3175	3185	3195	3205	3215	3225	3235	3245	3255	3265	3275	3285	3295	3305	3315	3325	3335	3345	3355	3365	3375	3385	3395	3405	3415	3425	3435	3445	3455	3465	3475	3485	3495	3505	3515	3525	3535	3545	3555	3565	3575	3585	3595	3605	3615	3625	3635	3645	3655	3665	3675	3685	3695	3705	3715	3725	3735	3745	3755	3765	3775	3785	3795	3805	3815	3825	3835	3845	3855	3865	3875	3885	3895	3905	3915	3925	3935	3945	3955	3965	3975	3985	3995	4005	4015	4025	4035	4045	4055	4065	4075	4085	4095	4105	4115	4125	4135	4145	4155	4165	4175	4185	4195	4205	4215	4225	4235	4245	4255	4265	4275	4285	4295	4305	4315	4325	4335	4345	4355	4365	4375	4385	4395	4405	4415	4425	4435	4445	4455	4465	4475	4485	4495	4505	4515	4525	4535	4545	4555	4565	4575	4585	4595	4605	4615	4625	4635	4645	4655	4665	4675	4685	4695	4705	4715	4725	4735	4745	4755	4765	4775	4785	4795	4805	4815	4825	4835	4845	4855	4865	4875	4885	4895	4905	4915	4925	4935	4945	4955	4965	4975	4985	4995	5005	5015	5025	5035	5045	5055	5065	5075	5085	5095	5105	5115	5125	5135	5145	5155	5165	5175	5185	5195	5205	5215	5225	5235	5245	5255	5265	5275	5285	5295	5305	5315	5325	5335	5345	5355	5365	5375	5385	5395	5405	5415	5425	5435	5445	5455	5465	5475	5485	5495	5505	5515	5525	5535	5545	5555	5565	5575	5585	5595	5605	5615	5625	5635	5645	5655	5665	5675	5685	5695	5705	5715	5725	5735	5745	5755	5765	5775	5785	5795	5805	5815	5825	5835	5845	5855	5865	5875	5885	5895	5905	5915	5925	5935	5945	5955	5965	5975	5985	5995	6005	6015	6025	6035	6045	6055	6065	6075	6085	6095	6105	6115	6125	6135	6145	6155	6165	6175	6185	6195	6205	6215	6225	6235	6245	6255	6265	6275	6285	6295	6305	6315	6325	6335	6345	6355	6365	6375	6385	6395	6405	6415	6425	6435	6445	6455	6465	6475	6485	6495	6505	6515	6525	6535	6545	6555	6565	6575	6585	6595	6605	6615	6625	6635	6645	6655	6665	6675	6685	6695	6705	6715	6725	6735	6745	6755	6765	6775	6785	6795	6805	6815	6825	6835	6845	6855	6865	6875	6885	6895	6905	6915	6925	6935	6945	6955	6965	6975	6985	6995	7005	7015	7025	7035	7045	7055	7065	7075	7085	7095	7105	7115	7125	7135	7145	7155	7165	7175	7185	7195	7205	7215	7225	7235	7245	7255	7265	7275	7285	7295	7305	7315	7325	7335	7345	7355	7365	7375	7385	7395	7405	7415	7425	7435	7445	7455	7465	7475	7485	7495	7505	7515	7525	7535	7545	7555	7565	7575	7585	7595	7605	7615	7625	7635	7645	7655	7665	7675	7685	7695	7705	7715	7725	7735	7745	7755	7765	7775	7785	7795	7805	7815	7825	7835	7845	7855	7865	7875	7885	7895	7905	7915	7925	7935	7945	7955	7965	7975	7985	7995	8005	8015	8025	8035	8045	8055	8065	8075	8085	8095	8105	8115	8125	8135	8145	8155	8165	8175	8185	8195	8205	8215	8225	8235	8245	8255	8265	8275	8285	8295	8305	8315	8325	8335	8345	8355	8365	8375	8385	8395	8405	8415	8425	8435	8445	8455	8465	8475	8485	8495	8505	8515	8525	8535	8545	8555	8565	8575	8585	8595	8605	8615	8625	8635	8645	8655	8665	8675	8685	8695	8705	8715	8725	8735	8745	8755	8765	8775	8785	8795	8805	8815	8825	8835	8845	8855	8865	8875	8885	8895	8905	8915	8925	8935	8945	8955	8965	8975	8985	8995	9005	9015	9025	9035	9045	9055	9065	9075	9085	9095	9105	9115	9125	9135	9145	9155	9165	9175	9185	9195	9205	9215	9225	9235	9245	9255	9265	9275	9285	9295	9305	9315	9325	9335	9345	9355	9365	9375	9385	9395	9405	9415	9425	9435	9445	9455	9465	9475	9485	9495	9505	9515	9525	9535	9545	9555	9565	9575	9585	9595	9605	9615	9625	9635	9645	9655	9665	9675	9685	9695	9705	9715	9725	9735	9745	9755	9765	9775	9785	9795	9805	9815	9825	9835	9845	9855	9865	9875	9885	9895	9905	9915	9925	9935	9945	9955	9965	9975	9985	9995	10005	10015	10025	10035	10045	10055	10065	10075	10085	10095	10105	10115	10125	10135	10145	10155	10165	10175	10185	10195	10205	10215	10225	10235	10245	10255	10265	10275	10285	10295	10305	10315	10325	10335	10345	10355	10365	10375	10385	10395	10405	10415	10425	10435	10445	10455	10465	10475	10485	10495	10505	10515	10525	10535	10545	10555	10565	10575	10585	10595	10605	10615	10625	10635	10645	10655	10665	10675	10685	10695	10705	10715	10725	10735	10745	10755	10765	10775	10785	10795	10805	10815	10825	10835	10845	10855	10865	10875	10885	10895	10905	10915	10925	10935	10945	10955	10965	10975	10985	10995	11005	11015	11025	11035	11045	11055	11065	11075	11085	11095	11105	11115	11125	11135	11145	11155	11165	11175	11185	11195	11205	11215	11225	11235	11245	11255	11265	11275	11285	11295	11305	11315	11325	11335	11345	11355	11365	11375	11385	11395	11405	11415	11425	11435	11445	11455	11465	11475	11485	11495	11505	11515	11525	11535	11545	11555	11565	11575	11585	11595	11605	11615	11625	11635	11645	11655	11665	11675	11685	11695	11705	11715	11725	11735	11745	11755	11765

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STATION EQUIPMENT

Grande Extension
New York
Yearly Aired

No.	Description of Equipment	Signals		Water (Gals)		Fuel (Gals)	Power (H.P.)	Capacity (Gals)	Grande Extension New York	Grande Extension New York	Dead End Ramp
		No.	Size	No.	Size						
	Interlocked	1									1
	No.	1		0	7,000		2				1
	No.	2		0	7,000		1				1
	No.	2		0	7,000		1	Capacity Supply from Mutual			1
	No.	2		0	7,000		2	1			1
	No.	2		0	7,000		2	1			1
	Interlocked	1		1	7,000		1		0.60	1	1
	Interlocked										1
	No.			2	7,000		4	1			1
	No.			2	7,000		5	2			1
	No.			2							1
	No.			2							1
	No.			2							1
	Not Interlocked	1			7,000		2	1			1

Registered No. of 5 ton crane tower - Serial # = 10397.

GRAND TOTALS OF COST OF BLDGS.

SHEET NO. 1

Structure	Type of Building	Drawing No.	Total cost of building	Cost charged as per bids	Cost of each building	Total cost per unit	Remarks
			SMS	SMS	SMS	SMS	
1. Barracks	314, 315, 316	2072/1-4	5000/-	3500/-	1500/-		Build. Be-partment ally.
2. 1001	"	"	4750/-	2500/-	1200/-		"
3. 1002	"	"	5000/-	3000/-	1500/-		"
4. 1003	"	"	5000/-	3000/-	1500/-		"
5. 1004	"	"	5000/-	3000/-	1500/-		"
6. 1005	"	"	5000/-	3000/-	1500/-		"
7. 1006	"	"	5000/-	3000/-	1500/-		"
8. 1007	"	"	5000/-	3000/-	1500/-		"
9. 1008	"	"	5000/-	3000/-	1500/-		"
10. 1009	"	"	5000/-	3000/-	1500/-		"
11. 1010	"	"	5000/-	3000/-	1500/-		"
12. 1011	"	"	5000/-	3000/-	1500/-		"
13. 1012	"	"	5000/-	3000/-	1500/-		"
14. 1013	"	"	5000/-	3000/-	1500/-		"
15. 1014	"	"	5000/-	3000/-	1500/-		"
16. 1015	"	"	5000/-	3000/-	1500/-		"
17. 1016	"	"	5000/-	3000/-	1500/-		"
18. 1017	"	"	5000/-	3000/-	1500/-		"
19. 1018	"	"	5000/-	3000/-	1500/-		"
20. 1019	"	"	5000/-	3000/-	1500/-		"
21. 1020	"	"	5000/-	3000/-	1500/-		"
22. 1021	"	"	5000/-	3000/-	1500/-		"
23. 1022	"	"	5000/-	3000/-	1500/-		"
24. 1023	"	"	5000/-	3000/-	1500/-		"
25. 1024	"	"	5000/-	3000/-	1500/-		"
26. 1025	"	"	5000/-	3000/-	1500/-		"
27. 1026	"	"	5000/-	3000/-	1500/-		"
28. 1027	"	"	5000/-	3000/-	1500/-		"
29. 1028	"	"	5000/-	3000/-	1500/-		"
30. 1029	"	"	5000/-	3000/-	1500/-		"
31. 1030	"	"	5000/-	3000/-	1500/-		"
32. 1031	"	"	5000/-	3000/-	1500/-		"
33. 1032	"	"	5000/-	3000/-	1500/-		"
34. 1033	"	"	5000/-	3000/-	1500/-		"
35. 1034	"	"	5000/-	3000/-	1500/-		"
36. 1035	"	"	5000/-	3000/-	1500/-		"
37. 1036	"	"	5000/-	3000/-	1500/-		"
38. 1037	"	"	5000/-	3000/-	1500/-		"
39. 1038	"	"	5000/-	3000/-	1500/-		"
40. 1039	"	"	5000/-	3000/-	1500/-		"
41. 1040	"	"	5000/-	3000/-	1500/-		"
42. 1041	"	"	5000/-	3000/-	1500/-		"
43. 1042	"	"	5000/-	3000/-	1500/-		"
44. 1043	"	"	5000/-	3000/-	1500/-		"
45. 1044	"	"	5000/-	3000/-	1500/-		"
46. 1045	"	"	5000/-	3000/-	1500/-		"
47. 1046	"	"	5000/-	3000/-	1500/-		"
48. 1047	"	"	5000/-	3000/-	1500/-		"
49. 1048	"	"	5000/-	3000/-	1500/-		"
50. 1049	"	"	5000/-	3000/-	1500/-		"
51. 1050	"	"	5000/-	3000/-	1500/-		"
52. 1051	"	"	5000/-	3000/-	1500/-		"
53. 1052	"	"	5000/-	3000/-	1500/-		"
54. 1053	"	"	5000/-	3000/-	1500/-		"
55. 1054	"	"	5000/-	3000/-	1500/-		"
56. 1055	"	"	5000/-	3000/-	1500/-		"
57. 1056	"	"	5000/-	3000/-	1500/-		"
58. 1057	"	"	5000/-	3000/-	1500/-		"
59. 1058	"	"	5000/-	3000/-	1500/-		"
60. 1059	"	"	5000/-	3000/-	1500/-		"
61. 1060	"	"	5000/-	3000/-	1500/-		"
62. 1061	"	"	5000/-	3000/-	1500/-		"
63. 1062	"	"	5000/-	3000/-	1500/-		"
64. 1063	"	"	5000/-	3000/-	1500/-		"
65. 1064	"	"	5000/-	3000/-	1500/-		"
66. 1065	"	"	5000/-	3000/-	1500/-		"
67. 1066	"	"	5000/-	3000/-	1500/-		"
68. 1067	"	"	5000/-	3000/-	1500/-		"
69. 1068	"	"	5000/-	3000/-	1500/-		"
70. 1069	"	"	5000/-	3000/-	1500/-		"
71. 1070	"	"	5000/-	3000/-	1500/-		"
72. 1071	"	"	5000/-	3000/-	1500/-		"
73. 1072	"	"	5000/-	3000/-	1500/-		"
74. 1073	"	"	5000/-	3000/-	1500/-		"
75. 1074	"	"	5000/-	3000/-	1500/-		"
76. 1075	"	"	5000/-	3000/-	1500/-		"
77. 1076	"	"	5000/-	3000/-	1500/-		"
78. 1077	"	"	5000/-	3000/-	1500/-		"
79. 1078	"	"	5000/-	3000/-	1500/-		"
80. 1079	"	"	5000/-	3000/-	1500/-		"
81. 1080	"	"	5000/-	3000/-	1500/-		"
82. 1081	"	"	5000/-	3000/-	1500/-		"
83. 1082	"	"	5000/-	3000/-	1500/-		"
84. 1083	"	"	5000/-	3000/-	1500/-		"
85. 1084	"	"	5000/-	3000/-	1500/-		"
86. 1085	"	"	5000/-	3000/-	1500/-		"
87. 1086	"	"	5000/-	3000/-	1500/-		"
88. 1087	"	"	5000/-	3000/-	1500/-		"
89. 1088	"	"	5000/-	3000/-	1500/-		"
90. 1089	"	"	5000/-	3000/-	1500/-		"
91. 1090	"	"	5000/-	3000/-	1500/-		"
92. 1091	"	"	5000/-	3000/-	1500/-		"
93. 1092	"	"	5000/-	3000/-	1500/-		"
94. 1093	"	"	5000/-	3000/-	1500/-		"
95. 1094	"	"	5000/-	3000/-	1500/-		"
96. 1095	"	"	5000/-	3000/-	1500/-		"
97. 1096	"	"	5000/-	3000/-	1500/-		"
98. 1097	"	"	5000/-	3000/-	1500/-		"
99. 1098	"	"	5000/-	3000/-	1500/-		"
100. 1099	"	"	5000/-	3000/-	1500/-		"
101. 1100	"	"	5000/-	3000/-	1500/-		"
102. 1101	"	"	5000/-	3000/-	1500/-		"
103. 1102	"	"	5000/-	3000/-	1500/-		"
104. 1103	"	"	5000/-	3000/-	1500/-		"
105. 1104	"	"	5000/-	3000/-	1500/-		"
106. 1105	"	"	5000/-	3000/-	1500/-		"
107. 1106	"	"	5000/-	3000/-	1500/-		"
108. 1107	"	"	5000/-	3000/-	1500/-		"
109. 1108	"	"	5000/-	3000/-	1500/-		"
110. 1109	"	"	5000/-	3000/-	1500/-		"
111. 1110	"	"	5000/-	3000/-	1500/-		"
112. 1111	"	"	5000/-	3000/-	1500/-		"
113. 1112	"	"	5000/-	3000/-	1500/-		"
114. 1113	"	"	5000/-	3000/-	1500/-		"
115. 1114	"	"	5000/-	3000/-	1500/-		"
116. 1115	"	"	5000/-	3000/-	1500/-		"
117. 1116	"	"	5000/-	3000/-	1500/-		"
118. 1117	"	"	5000/-	3000/-	1500/-		"
119. 1118	"	"	5000/-	3000/-	1500/-		"
120. 1119	"	"	5000/-	3000/-	1500/-		"
121. 1120	"	"	5000/-	3000/-	1500/-		"
122. 1121	"	"	5000/-	3000/-	1500/-		"
123. 1122	"	"	5000/-	3000/-	1500/-		"
124. 1123	"	"	5000/-	3000/-	1500/-		"
125. 1124	"	"	5000/-	3000/-	1500/-		"
126. 1125	"	"	5000/-	3000/-	1500/-		"
127. 1126	"	"	5000/-	3000/-	1500/-		"
128. 1127	"	"	5000/-	3000/-	1500/-		"
129. 1128	"	"	5000/-	3000/-	1500/-		"
130. 1129	"	"	5000/-	3000/-	1500/-		"
131. 1130	"	"	5000/-	3000/-	1500/-		"
132. 1131	"	"	5000/-	3000/-	1500/-		"
133. 1132	"	"	5000/-	3000/-	1500/-		"
134. 1133	"	"	5000/-	3000/-	1500/-		"
135. 1134	"	"	5000/-	3000/-	1500/-		"
136. 1135	"	"	5000/-	3000/-	1500/-		"
137. 1136	"	"	5000/-	3000/-	1500/-		"
138. 1137	"	"	5000/-	3000/-	1500/-		"
139. 1138	"	"	5000/-	3000/-	1500/-		"
140. 1139	"	"	5000/-	3000/-	1500/-		"
141. 1140	"	"	5000/-	3000/-	1500/-		"
142. 1141	"	"	5000/-	3000/-	1500/-		"
143. 1142	"	"	5000/-	3000/-	1500/-		"
144. 1143	"	"	5000/-	3000/-	1500/-		"
145. 1144	"	"	5000/-	3000/-	1500/-		"
146. 1145	"	"	5000/-	3000/-	1500/-		"
147. 1146	"	"	5000/-	3000/-	1500/-		"
148. 1147	"	"	5000/-	3000/-	1500/-		"
149. 1148	"	"	5000/-	3000/-	1500/-		"
150. 1149	"	"	5000/-	3000/-	1500/-		"
151. 1150	"	"	5000/-	3000/-	1500/-		"
152. 1151	"	"	5000/-	3000/-	1500/-		"
153. 1152	"	"	5000/-	3000/-	1500/-		"
154. 1153	"	"	5000/-	3000/-	1500/-		"
155. 1154	"	"	5000/-	3000/-	1500/-		"
156. 1155	"	"	5000/-	3000/-	1500/-		"
157. 1156							

UNITED STATES - COST OF BUILDINGS

SHEET NO. 1

Station.	Type of Building.	Drawing No.	Total cost of building materials in U.S.	Total cost of labor in building. SHE	Cost per unit. SHE	Remarks.
Remain.	2 Unit Asstic Qrs.	476 Rev.	15,041/-	8,741/-	7,520/-	Build. Depara-mentally.
Remain.	2 Unit Asstic Qrs.	1957	9,000/-	8,852/-	8,776/-	"
Total cost of Asstic Quarters (Uranda) 528, 115/-						Average cost of one 2 Unit Asstic Qrs. - She. 6,836/-

U.S. & A.M. A. - CONTINUED.

18

GENERAL INVESTMENT BUILDINGS

SHEET NO. 10

Location	Floor or Building	Building No.	Total Cost of Building	Cost of Part in this Building	Amount of Investment in Building	Total Cost of Building
Four	2nd, 3rd, 4th	1-2	\$1,100,000/-	75,000/-	535,000/-	535,000/-
Six	3rd, 4th, 5th	3	1,000,000/-	750,000/-	700,000/-	700,000/-
One	1st	4	300,000/-	50,000/-	150,000/-	150,000/-
One	1st	5	1,500,000/-	1,000,000/-	1,000,000/-	1,000,000/-
One	1st	6	1,500,000/-	1,000,000/-	1,000,000/-	1,000,000/-

Built by
Contractor

The following scale of rations was adhered to in Kenya:-

SCALE OF RATIONS.

- (a) Rice 2 lbs. per diem
- (b) Lentils..... 1 " " "
- (c) Chiroko beans..... 1 " " "
- (d) Ground nuts or Shea..... 1 lb. per week or
3 Ouz. " "
- (e) Fresh vegetables..... 1 lb.
- (f) Salt..... 3 Ouz. " "

ALTERNATIVE RATIONS.

- In lieu of (b) 4 Ouz. of Chiroko beans per diem or
- 2 " " Jaggery " " "
 - 2 " " Ground nuts " " "
 - " " (c) 1 " " Beans " " "
 - 2 " " Jaggery " " "
 - " " (e) 2 Lemons per week

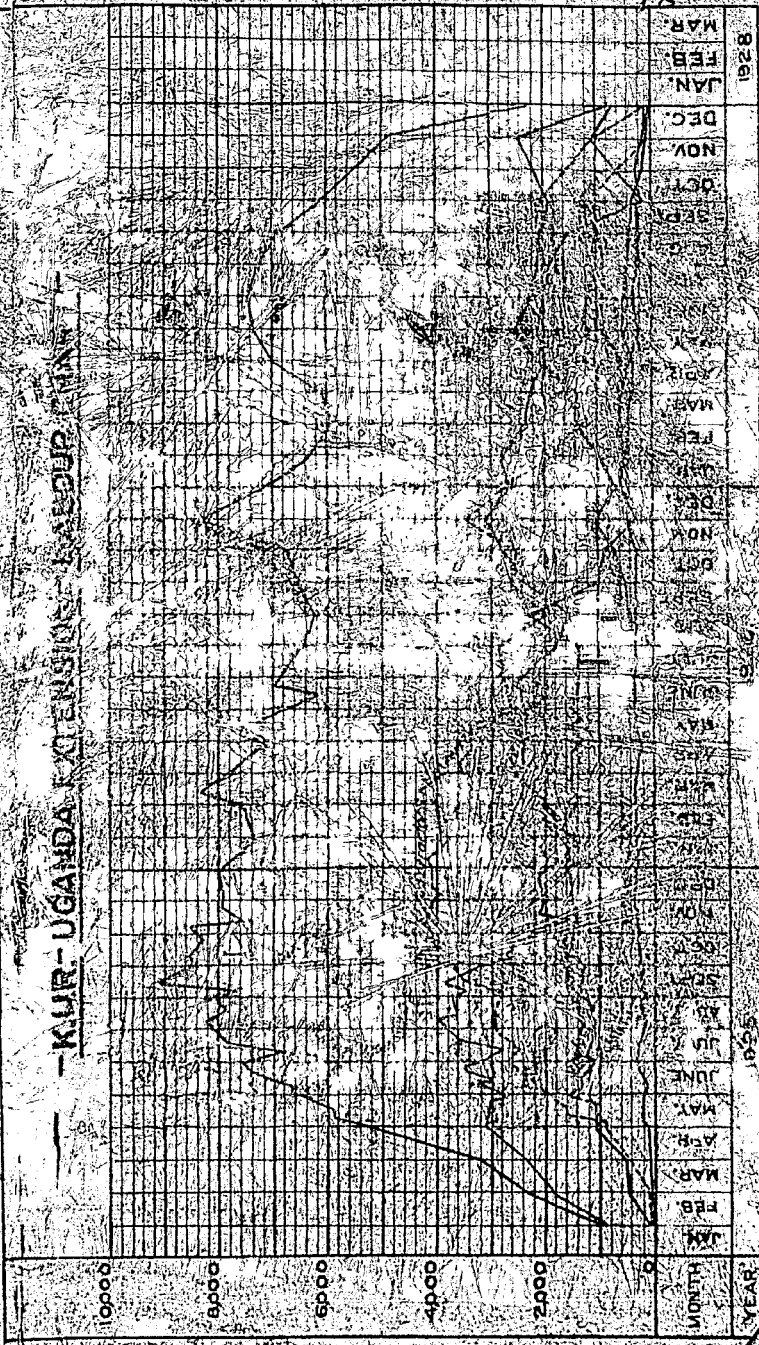
The alternative rations was only issued when it was impossible to obtain other ingredients as stated.

The following is the scale of Rations adhered to in
Uganda:

SCALE OF RATIONS.

Maize Meal.....	10 1/2 lbs.	per week.
BEANS.....	4 "	" "
Potatoes.....	1 "	" "
Salt.....	2 Ozs.	" "

-KUR-UGANDA EXTENSION-1-ABDUP-1955-



82

8261

1955

CONTRACT NO. K 7 BEYOND BRODERICK FALLS

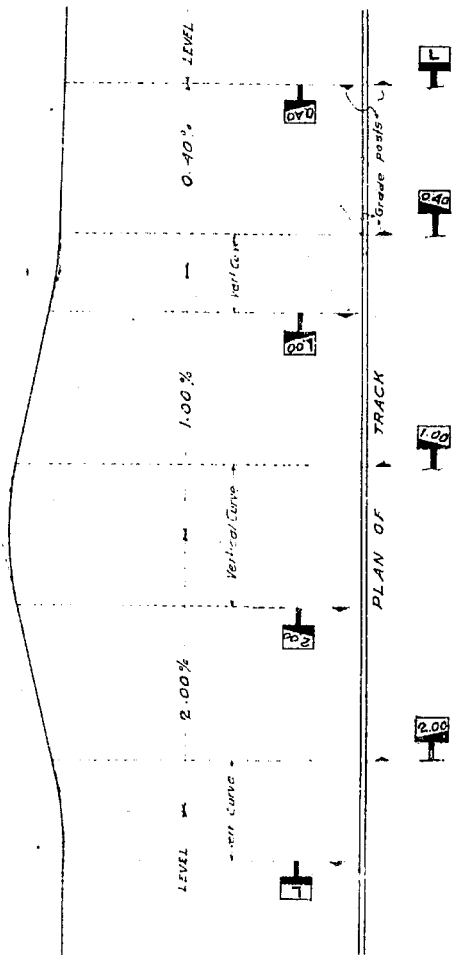
Col No	QTY	UNIT PRICE	TOTAL	PLACING CONTRACT COST	SUPPLY BALLAST SAND COST	TOTAL COST, NOT INCLUDING COST OF EXPEN	RATE PER 100 C.Y.	
	1771	4960.75	7320	13762.00		6500.40	100/70	Contract K. 7
	1007	1053.40		2788.00		3994.40	100/40	
	2312	6781.50		3279.00		9050.50	101/00	Includes cost sand, stone
	2287	2678.40	75.20	3098.00		3737.60	102/00	transport on shuttering
	1129	1384.50	56.00	3050.00		4467.50	102/20	& cement from Turbo
	1207	2708.40	61	6406.50		9217.50	100/00	
	1207	2708.40	61	6406.50	1306.00	11742.20	125/00	Contract K. 25 Transport 3 miles sand special difficulties in transporting cement from turbo-heavy rains
	1087	600.50		9400.00		10750.00	190/75	Contract K. 10 includes cost transport of cement - Hiram to Valicha-Shuttering Hoist
	8405	100.00		1698.00		18425.00	189/50	to Valicha
				12360.00	train	12360.00	100/-	Contract K. 65 Contractor supplies ballast and sand - provided labour loading & offloading
				10712.00		20712.00	160/-	
				1220.00		3320.00	100/-	
				7889.00	1639.00	9527.00	120/50	Contract K. 63 Contractor was supplied with ballast & sand Sept above here
				7476.50	1450.00	8906.50	95/30	Contract K. 78

K.U.R.
 UGANDA EXTENSION
 GRADE POSTS
 PAGE 4

A.D. Dempster
 Resident Engineer



5 Figures
 Posts



Concrete Grade Post

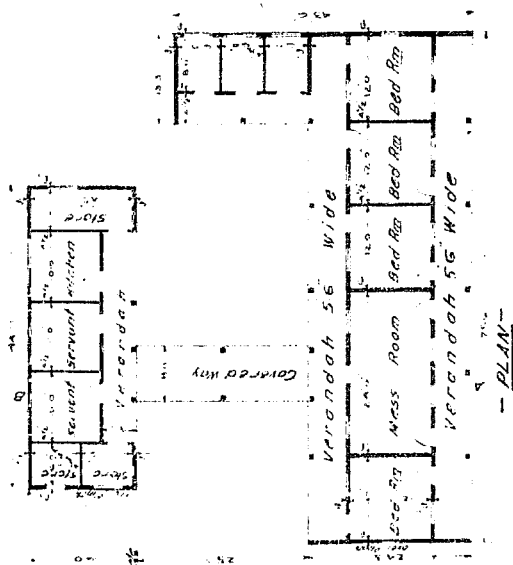
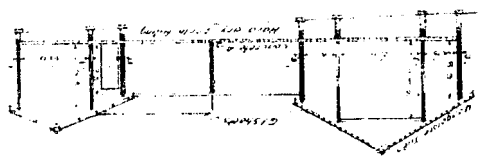
- K. U. R. -
 - UGANDA EXTENSION -
 - ASIATIC RUNNING ROOM -
 - AT TORORO STATION -

A. H. Campbell
 Resident Engineer

See also...
 ...

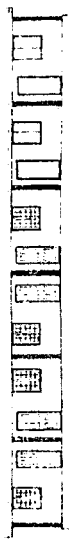
- Date -

- CROSS SECTION A-A -



- PLAN -

Maximum width of the Road



- FRONT ELEVATION -

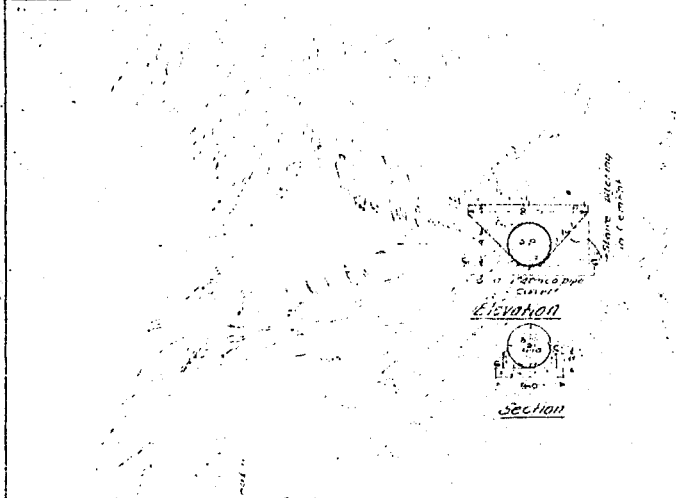
— K. U. R. —
 UGANDA EXTENSION
 30 INCH PIPE CULVERT
 S.C.P. 2

Handwritten signature

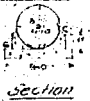
THE BOARDERS WORK
 IS ACTUALLY CONSTRUCTED

DAYS

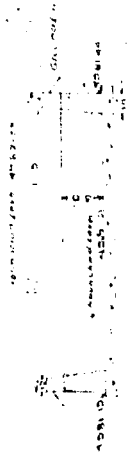
N^o / MILEAGE
 VARIOUS



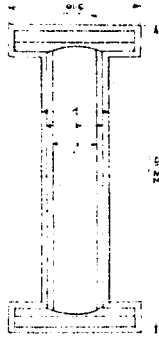
Elevation



Section



Longitudinal Section



22' 0"

Plan

K.U.R.

UGANDA EXTENSION
TYPE HEAD WALLS FOR 4" VC PIPE

3" DIA. PIPE

Handwritten signature
25 and 26 pages



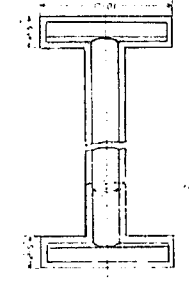
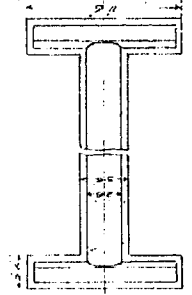
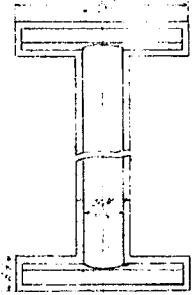
3.0 ARMCO PIPE HEADWALLS



2.6 ARMCO PIPE HEADWALLS



2.0 ARMCO PIPE HEAD WALLS



Along flange in cement



Along flange in cement



No / MILEAGE VARIOUS

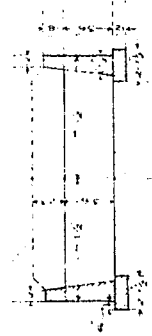
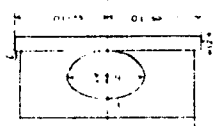
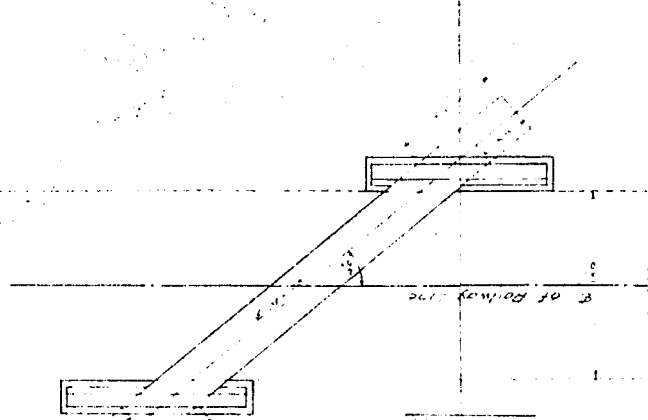
K.U.R.

UGANDA EXTENSION
3 CONCRETE PIPE IN
TULBO GODOWN SIDING

H. J. ...
...

... ..
... ..

6/12



SECTION

No 1A

MILEAGE
146/12

K. S. U.R.

UGANDA EXTENSION
3/6 X 4 ARCH CULVERT

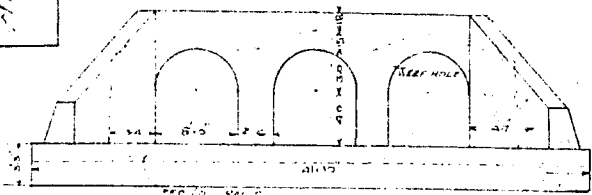
Scale 1" = 8 feet

Prepared by
Resident Engineer

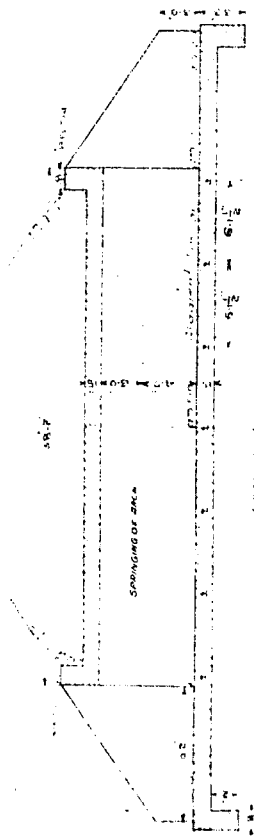
THIS DRAWING IS ONE COPY
AN ACTUALLY CONSTRUCTED

1947

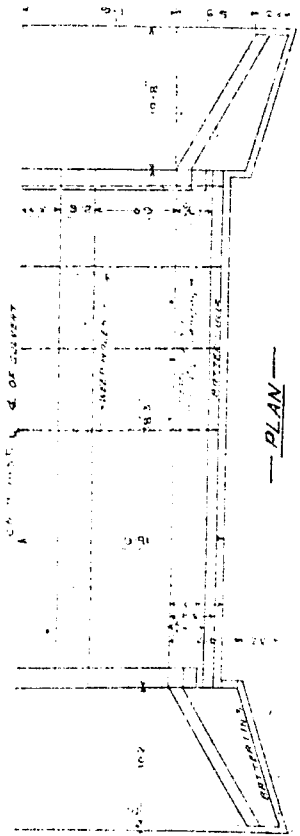
MILEAGE
No 2 147/5



ELEVATION



LONGITUDINAL SECTION



PLAN

ORIGINATOR: JAMES DOUGLAS

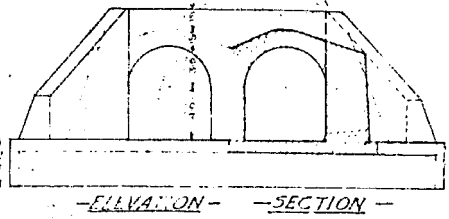
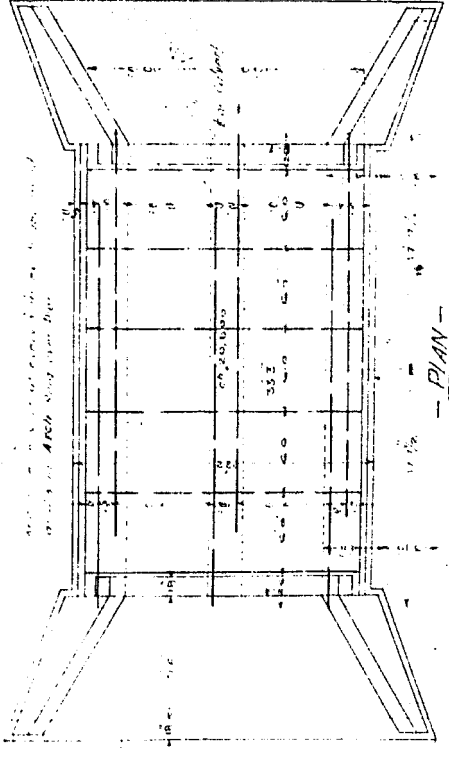
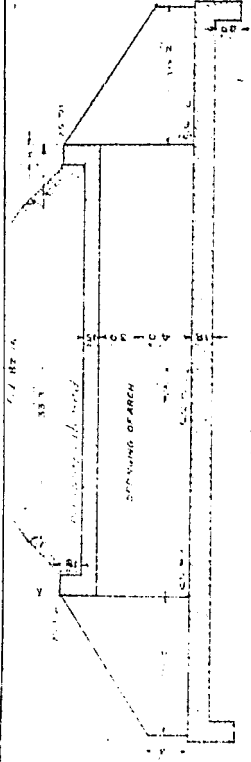
K & U R
 UGANDA EXTENSION
 SIXTH ARCH CULVERT

Scale 1/8" = 1'-0"
 H. J. [Signature]
 Resident Engineer

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

No 5 MILEAGE 150/8

As shown on drawing of bridge
 and culvert on page 4
 complete and arch and culvert
 & abutments and 2.5 x 1.5 ft. arch
 & arch, location found complete



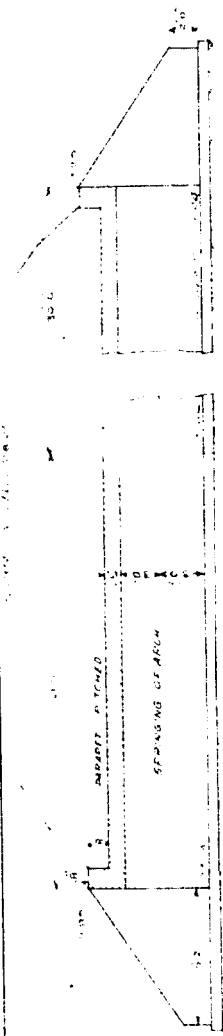
DATE

K & U R
 UGANDA EXTENSION
 SYRARCH CULVERT

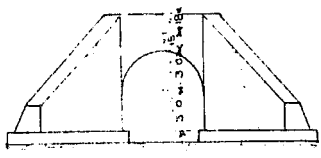
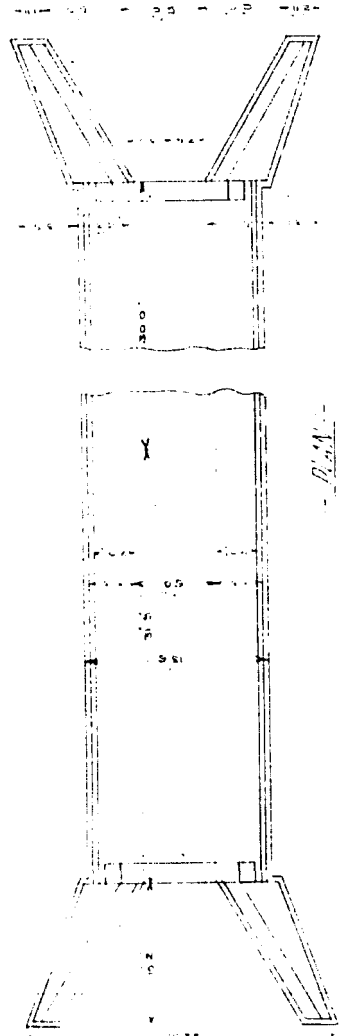
SCM Contract
 J. J. Gwynne
 Resident Engineer

1:15 DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

DATE



— LAYOUT IN PLAN —



No 7 MILEAGE
 151/11

K.U.R.

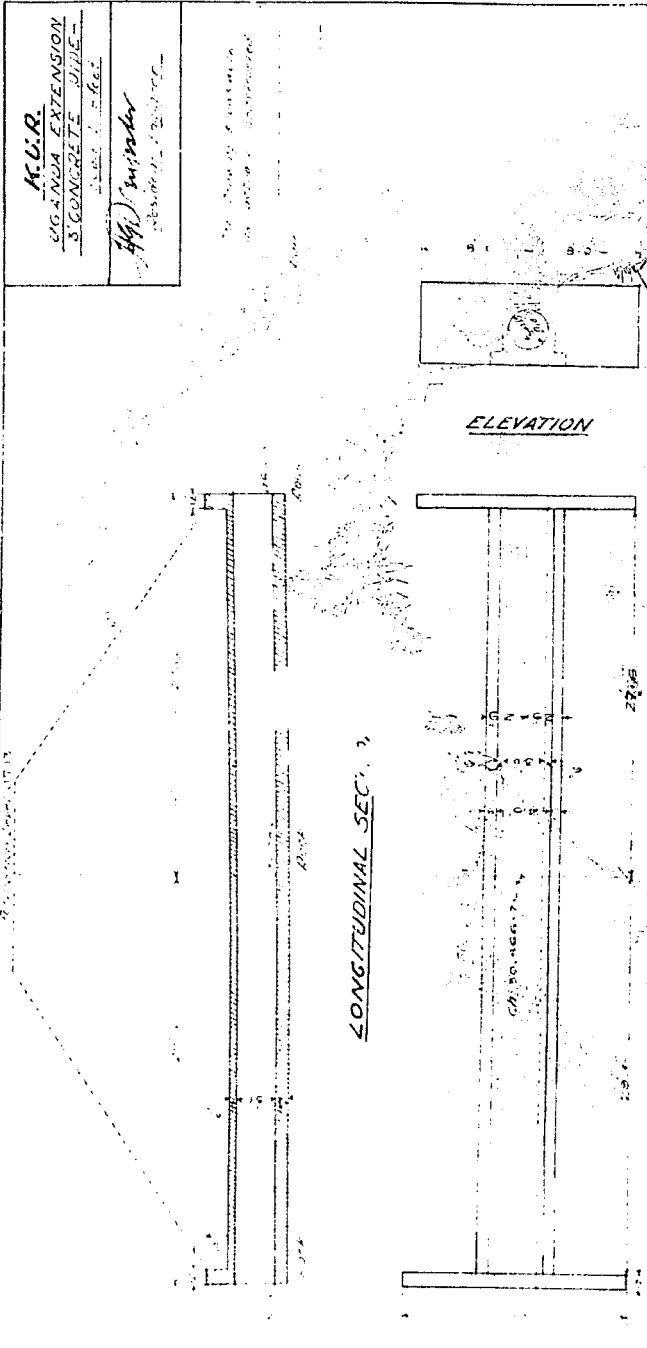
SECOND EXTENSION
CONCRETE PIPE

200' x 1' - 6 1/2"

W.D. Minter

APPROVED: [Signature]

THE TOWN OF [unclear]
BY [unclear]



ELEVATION

LONGITUDINAL SEC.

PLAN

MILEAGE
N^o 7A
152/5

K. & U.R.

**LIGANDA EXTENSION
116 X 1 ARCH CULVERT**

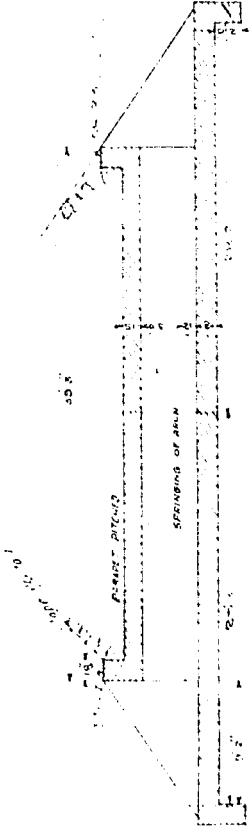
Scale 1" = 8 Feet

E. J. Murphy
Resident Engineer

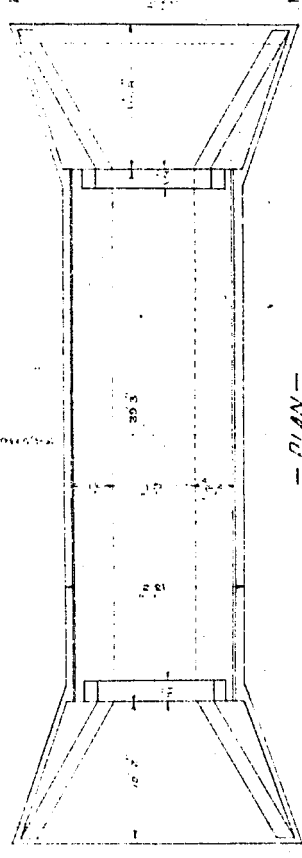
THIS DRAWING SHOWS WORK
AS ACTUALLY CONSTRUCTED

No 9
MILEAGE
153/3

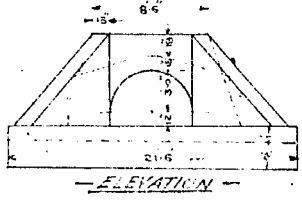
CONCRETE ARCH



TRANSVERSAL SECTION



PLAN



ELEVATION

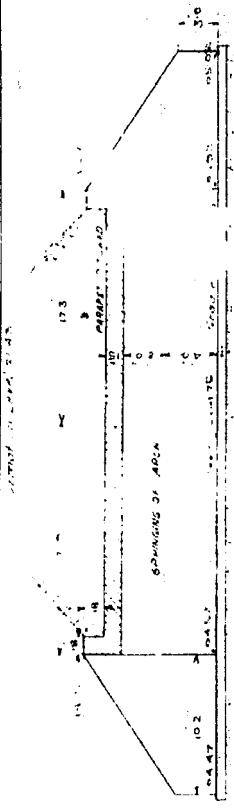
ART.

K.&U.R.
LIGANDA EXTENSION
6-11 ARCH CULVERT
 Scale 1" = 8' F.T.
 Resident Engineer

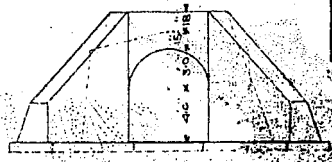
THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

DATE

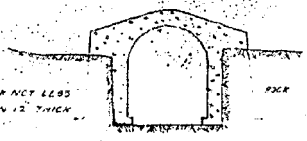
No 10 MILEAGE
 154/1



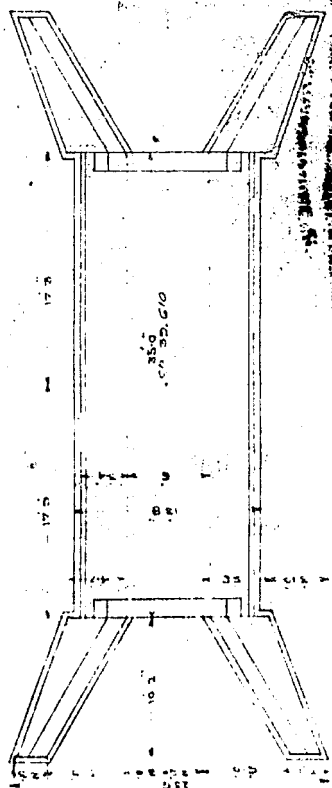
LONGITUDINAL SECTION



ELEVATION



PLAN

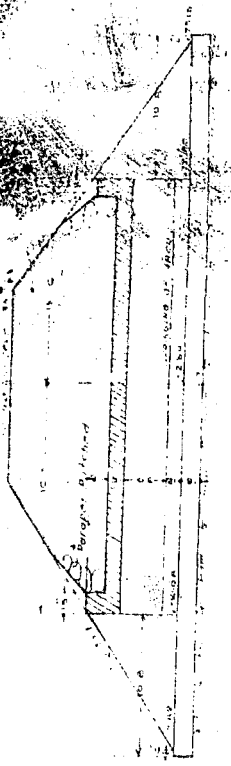


& U.R.
 UGAY DA EXTENSION
 GKI ARCH COVERT
 SCALE 1/8"=1'-0"

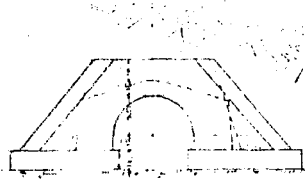
H. S. ...
 ... Engineer

Drawing ...
 as Actually Constructed

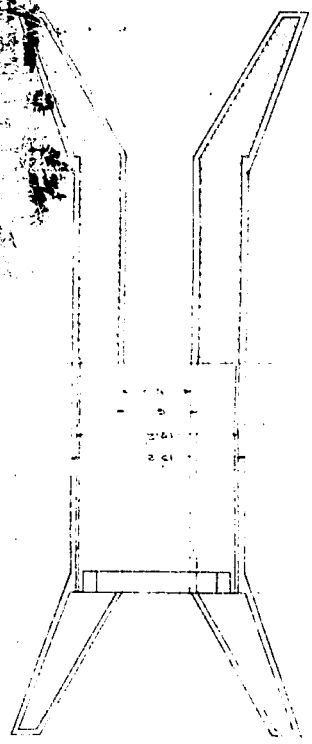
DATE



LONGITUDINAL SECTION



ELEVATION



PLAN AT FLOOR LEVEL

No II
 MILEAGE
 154/20

H. & U. R.

**UGANDA EXTENSION
12x8 ARCH CULVERT**

Scale 1" = 8 Feet

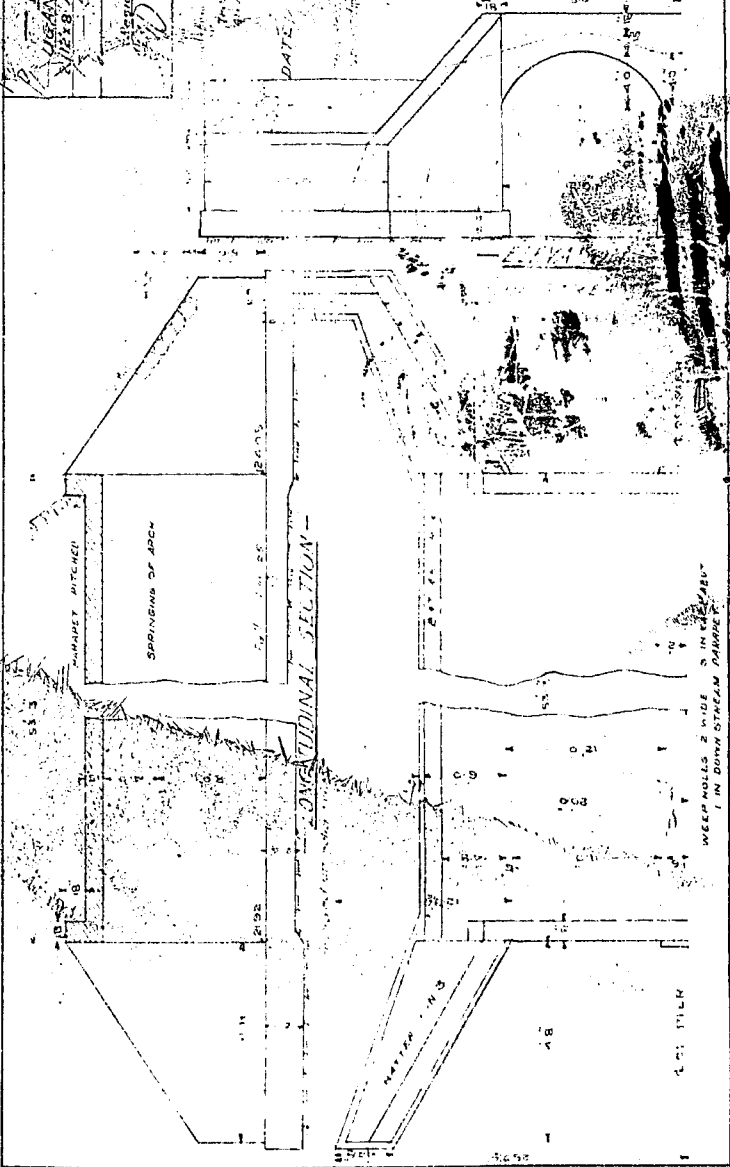
Assistant Engineer

H. P. Mitchell

THIS DRAWING SHOWS WORK
AS ACTUALLY CONSTRUCTED

DATE

MILEAGE
No 13 155/19



WEEP HOLES 2 WIDE 3 IN LENGTH
1 IN BOTH STREAM BANKS

1:10:10

K & U.R.
 UGANDA EXTENSION
 6 x 3 ARCH CULVERT
 Scale 1" = 8' 0"

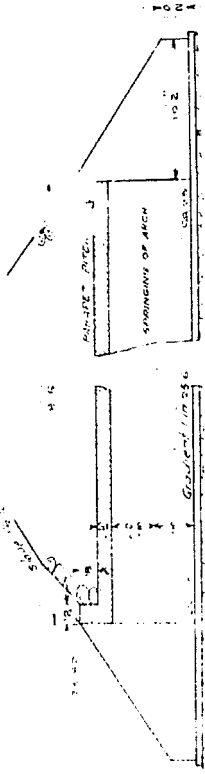
Resident Engineer
H. D. Campbell

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

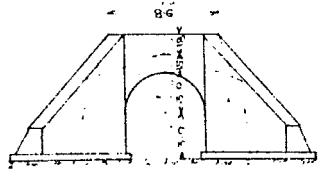
No 15
 MILEAGE
 157/1

DATE

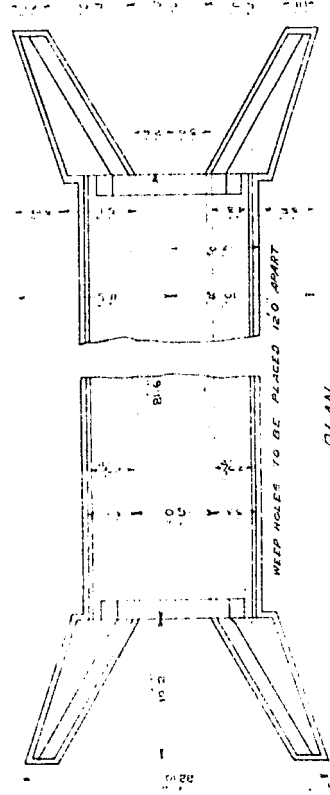
1/24/20/21



— LONGITUDINAL SECTION —



— ELEVATION —



— PLAN —

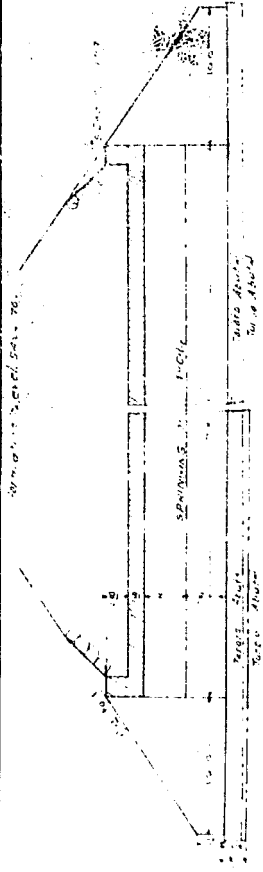
— K. U. R. —
 UGANDA EXTENSION
 6 x 3 ARCH CULVERT —
 — scale 1" = 8'00" —

W. J. M. ...
 Resident Engineer

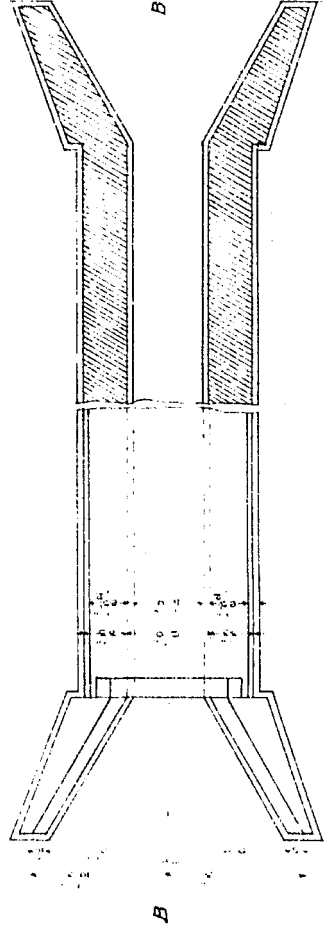
This Drawing Shows
 Work as Actually
 Constructed.

Date.

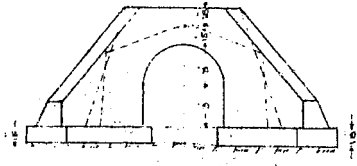
No 17 MILEAGE
 158/18



— Section B.B. —



— Plan of Floor Level. —



— Elevation —

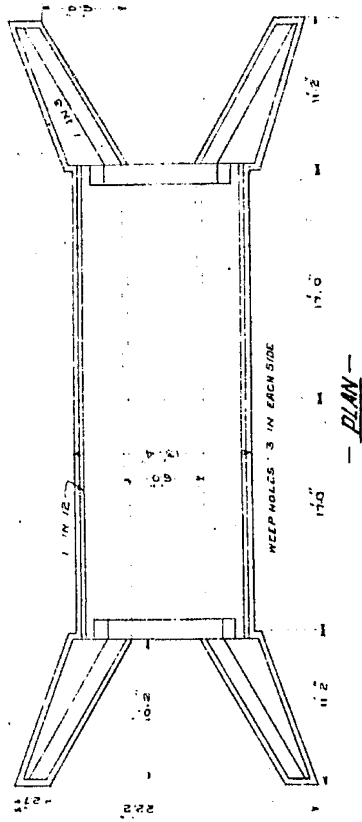
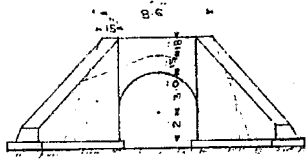
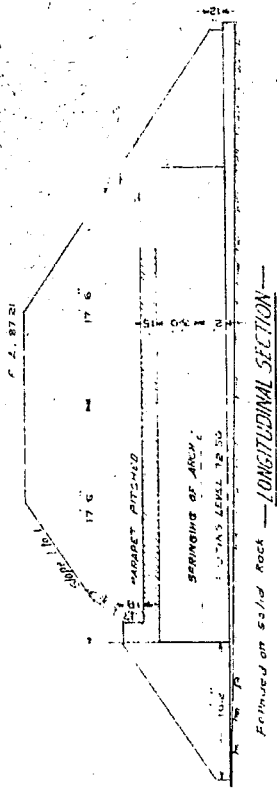
A & U P
 UGANDA EXTENSION
 6 1/2 ARCH CULVERT

Scale: 1" = 8' 0"
 Resident Engineer
J. H. Dwyer

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

DATE

No 20 MILEAGE
 161/21

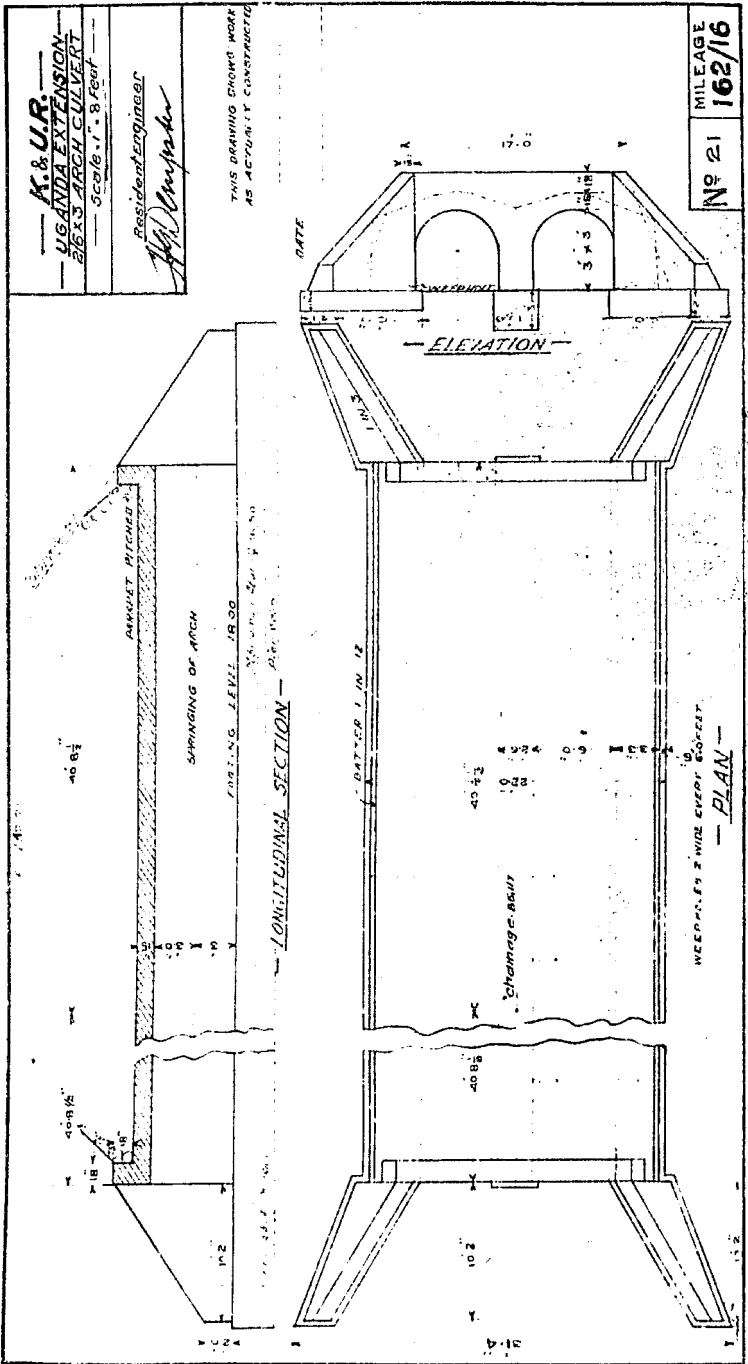


K. & U.R.
 US ANNA EXTENSION
 SIX ARCH CULVERT
 Scale 1" = 8 Feet

Resident Engineer
H. D. Campbell

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

NO 21 MILEAGE
 162/16



WEED P.S. 2 WIDE EVERY 60 FEET

K.U.R.

**UGANDA EXTENSION
BRIDGE OVER KIPKARREN R.**

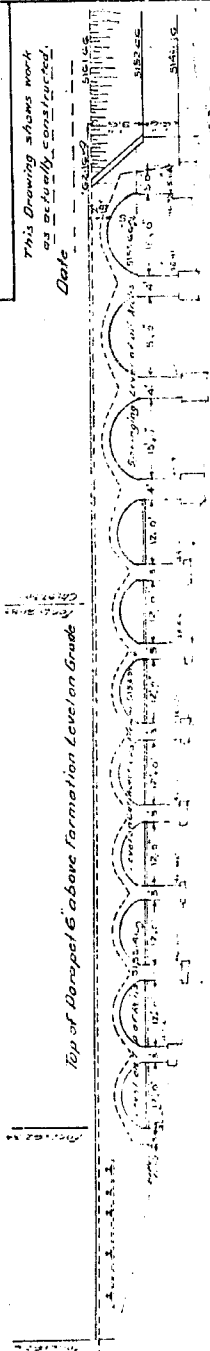
Scale: 1/20 feet

A. J. Simpson
Resident Engineer

This Drawing shows work
as actually constructed.
Date _____

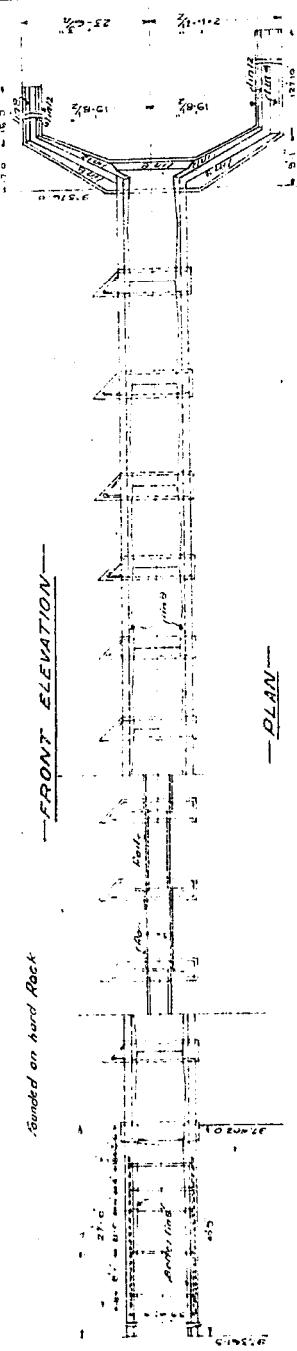
North Arrow

Top of Danopal's above Formation Level on Grade



FRONT ELEVATION

founded on hard Rock



PLAN

No. 23

MILEAGE
165/1

K. & U.R.
 UGANDA EXTENSION
 6x2 ARCH CULVERT

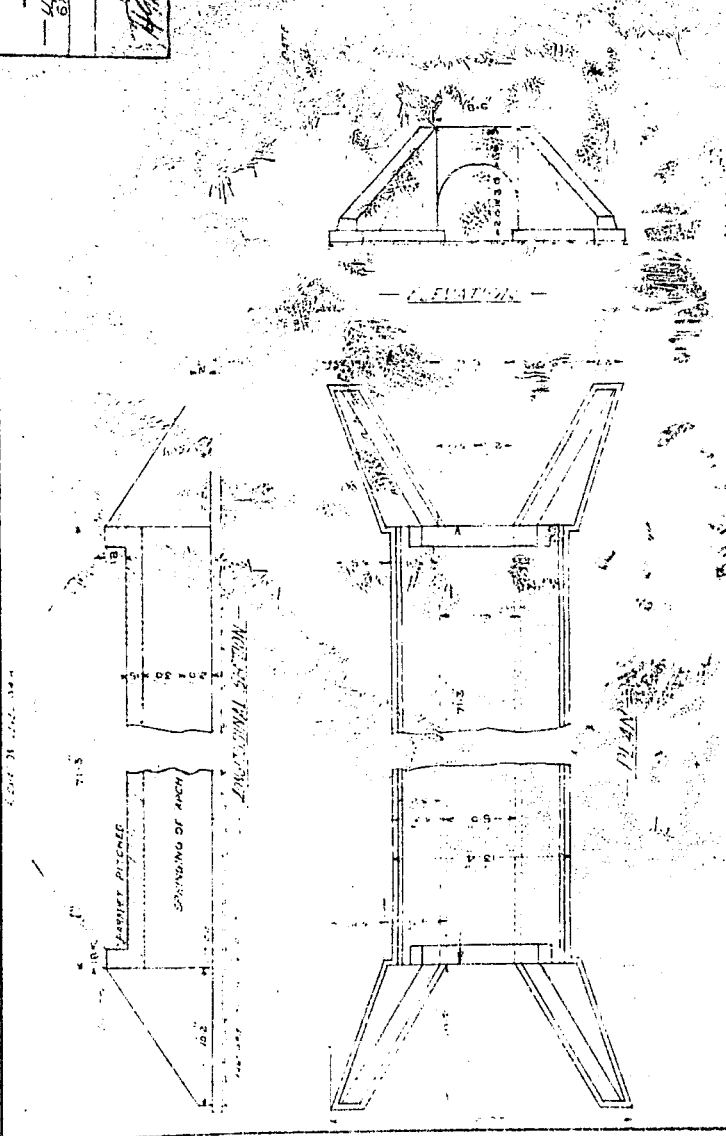
Scale 1" = 8' feet

Resident Engineer

H. H. Campbell

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

No 24 MILEAGE 166/1



K. & U. R.

UGANDA EXTENSION
2 1/2 x 5 ARCH CULVERT

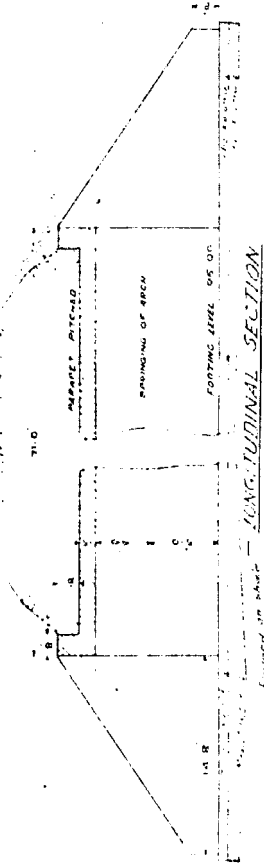
Scale: 1" = 8 Feet

Assistant Engineer

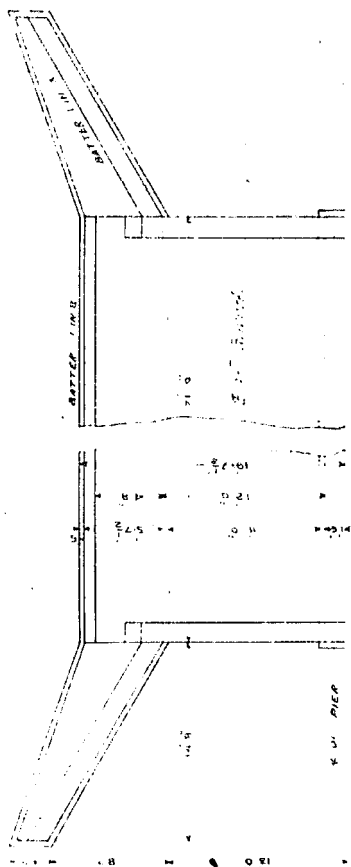
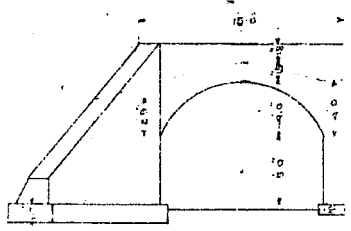
A. M. Mphahle

THIS DRAWING SHOWS WORK AS
ACTUALLY CONSTRUCTED

MILEAGE
No 26 168/12



DATE



WEIGHABLES 5' IN EACH ABUT. AND 3' TRANSITION
ARCH RING

K & U.R.

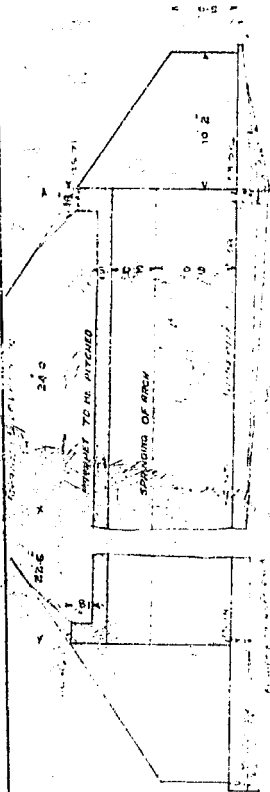
— LOMBA EXTENSION —
— 6X6 ARCH CULVERT —

Scale 1/2" = 1'-0"

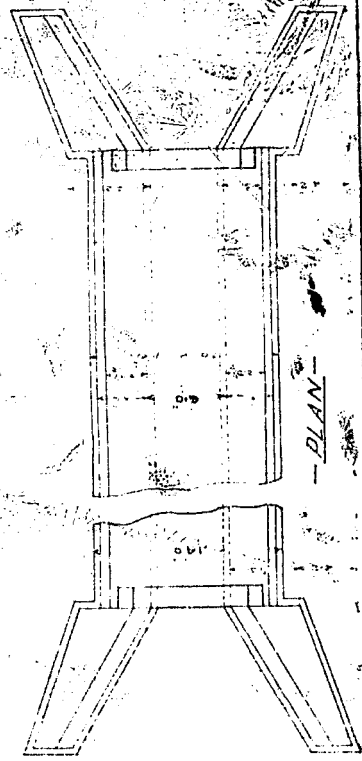
Approved Engineer

THIS DRAWING SHOWS HOW
AS ACTUALLY CONSTRUCTED

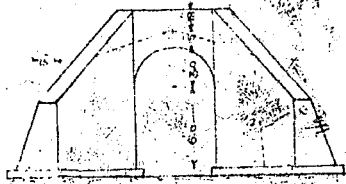
№ 27 MILEAGE
169/11



LONGITUDINAL SECTION



PLAN



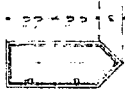
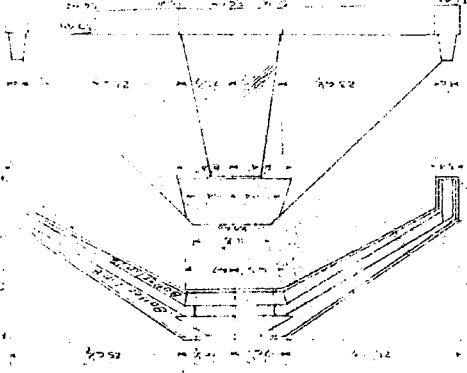
ELEVATION

- K. U. R. -
 - LICANDA EXTENSION -
 - 250 METER SPAN BRIDGE -
 - OVER AZUOLA RIVER -

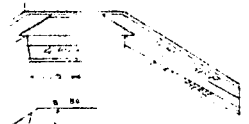
Off. Construction

NO. VILLAGI
 28/170/12

- ELEVATION OF MILLABUT -



- PLAN -



VAL. N. TO AB.

K. & U.R.
 LIGANDA EXTENSION
 3/12X6 ARCH CULVERT

Scale 1" = 8' Feet

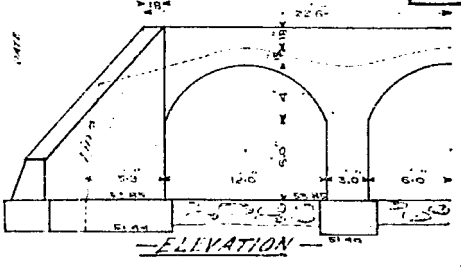
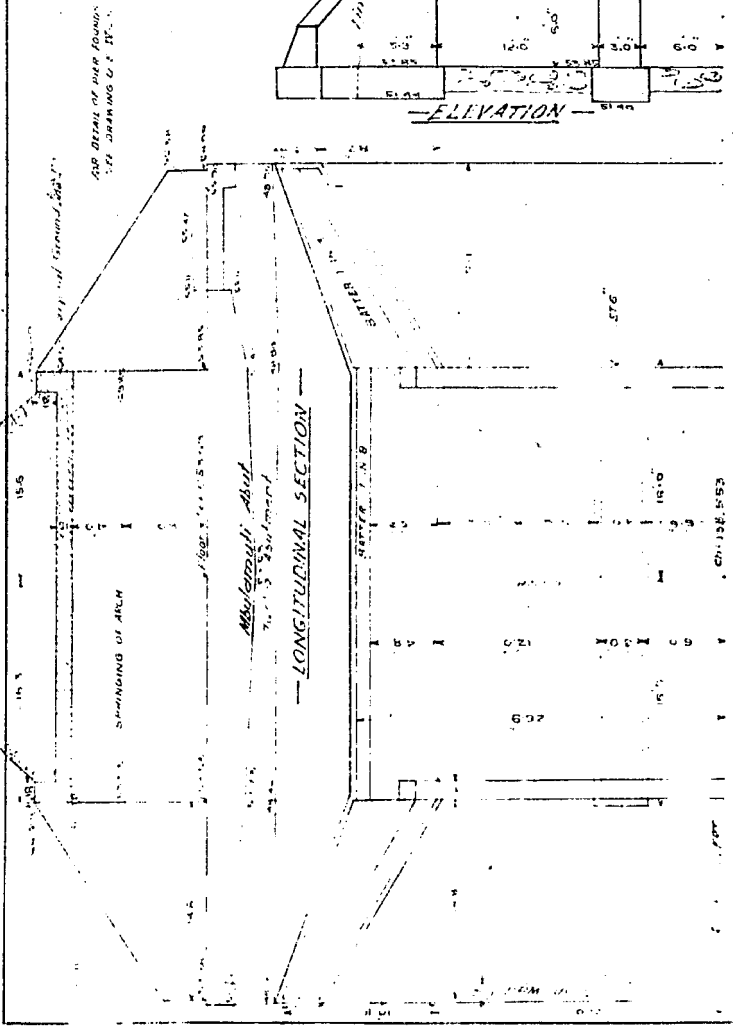
Resident Engineer

H. D. Cooper

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

No 29 MILEAGE
 171/18

Longitudinal Level 19.56



PLAN

K. & U.R.
 UGANDA EXTENSION
 SIX ARCH CULVERT

Scale 1" = 8 Feet

Resident Engineer

A. D. Omisaka

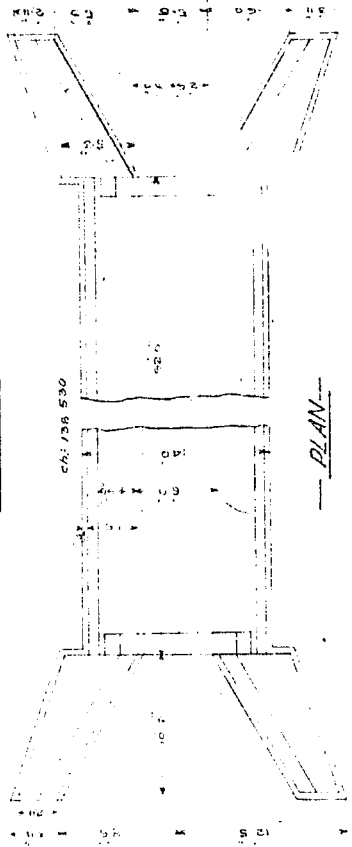
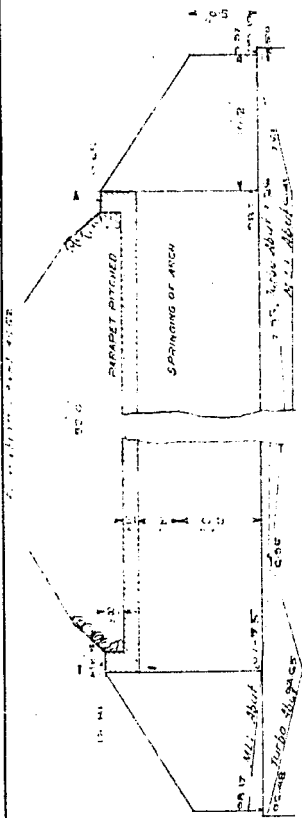
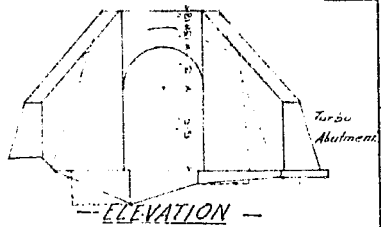
THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

Nº 31

MILEAGE
 172/18

Abutment
 Abutment

NOTE



-K.U.R.-
-UGANDA EXTENSION-
-5 x 8 ARCH CULVERT-
-Scale: 1" = 16 Feet-

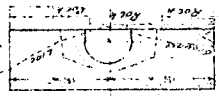
Resident Engineer
[Signature]

This drawing shows work
 as actually constructed

Date

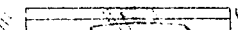
Nº 32 **MILEAGE**
173/2

-ELEVATION Upstream-



1" = 16 Feet

4952.56



-ELEVATION Parallel to C.C.-



Line of Railway

- PLAN -



-ELEVATION Downstream-

A. & U.R.
 UGANDA EXTENSION
 3.3' RICH CULVERT

Scale 1" = 8 Feet

Resident Engineer
[Signature]

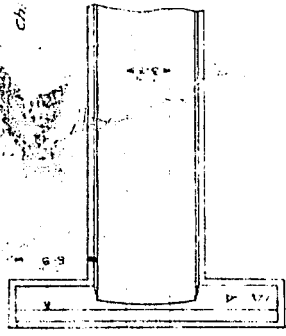
THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

MILEAGE
 173/11
 No 33

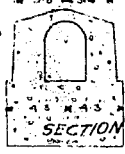


SECTION —

Ch. 32.060
 38 6



ELEVATION —



At Centre of Culvert.

PLAN —

-- KUR --
 -- UGANDA EXTENSION --
 -- I-40 GIRDER SPAN BRIDGE --
 -- OVER KUIWAR RIVER --

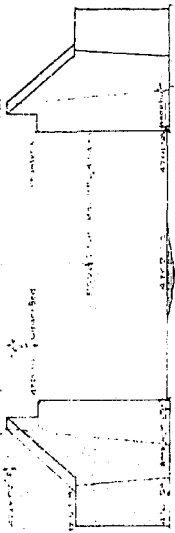
300.00 66' 1 1/2 inch
 Mr. [Signature]
 PRESIDENT, INC. VFLP

THIS DRAWING SHOWN AS
 ACTUALLY CONSTRUCTED

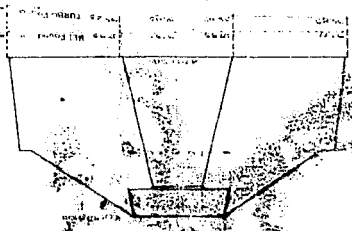
NO 35 MILEAGE
 182/19

ELEVATION OF ABUTMENT

DATE



-- ELEVATION --



-- PLAN --

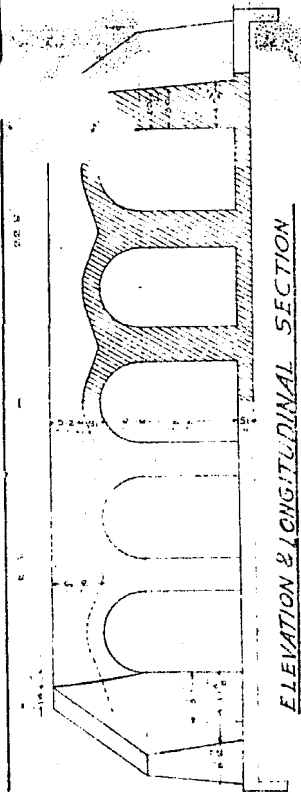


-- FOUNDATION --

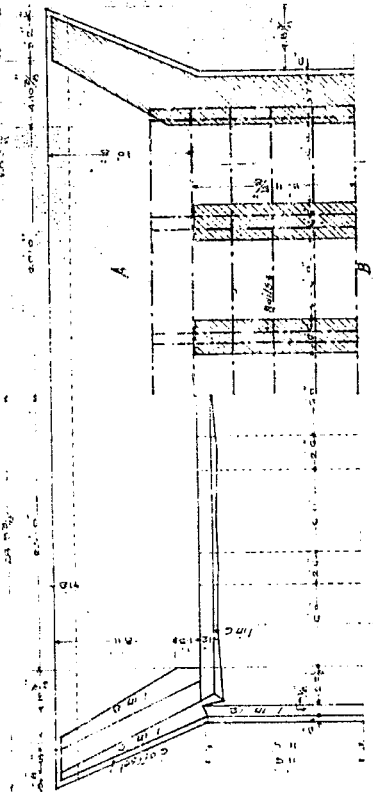
— **K & U R** —
 — **UGANDA EXTENSION** —
 — **SIGTARCH CULVERT** —
 — **OVER BOKKOLLE RIV** —
 ST. 216.11.80
 H.A. Resident Engineer —

This drawing shows work
 not actually constructed

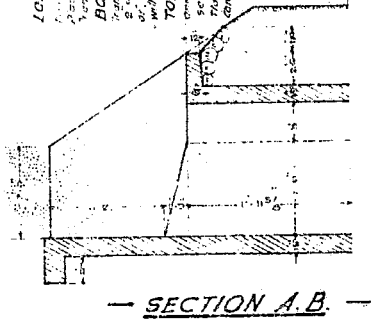
DATE



ELEVATION & LONGITUDINAL SECTION



— PLAN AT FLOOR LEVEL —



— SECTION A.B. —

LONGITUDINAL RAILS.
 These rails are to be riveted
 to the top of the concrete
 transverse rails.

BOTTOM TRANSVERSE RAILS
 These rails are to be riveted
 to the base of the concrete
 transverse rails, and to be
 spaced at 2 feet between ends of rails.

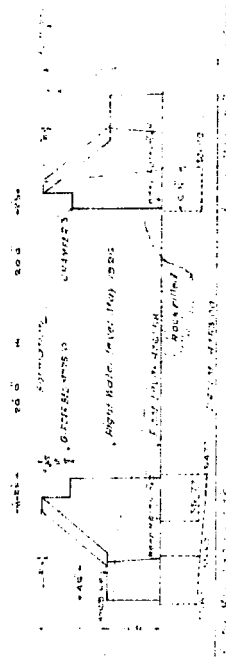
TOP TRANSVERSE RAILS
 These rails are to be riveted
 to the top of the concrete
 transverse rails, and to be
 spaced at 2 feet between ends of rails.

№ 36 **MILEAGE**
183/11

— KUR —
 — UGANDA EXTENSION —
 — 140 GIRDER SPAN BRIDGE —
 — OVER CHUERE RIVER —
 — Scale 1/6" = 1' —
 — Resident Engineer —

U.S. 2-24-48 10' 11" 1/2 4000 45
 P.C. 100' 0" 10' 11" 1/2 4000 45

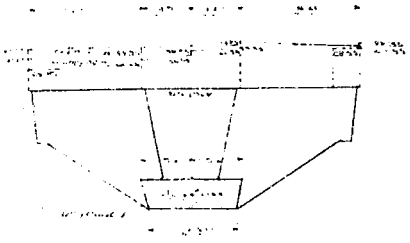
No 37
 MILEAGE
 187/2



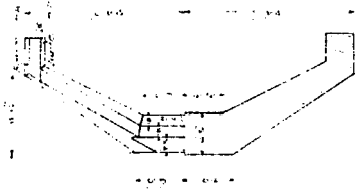
— ELEVATION —

— DATE —

— ELEVATION OF ABUTMENT —



— PLAN —



— FOUNDATION PLAN —

K. & U. R.

LIGANDA EXTENSION
3/12 X 6 ARCH CULVERT

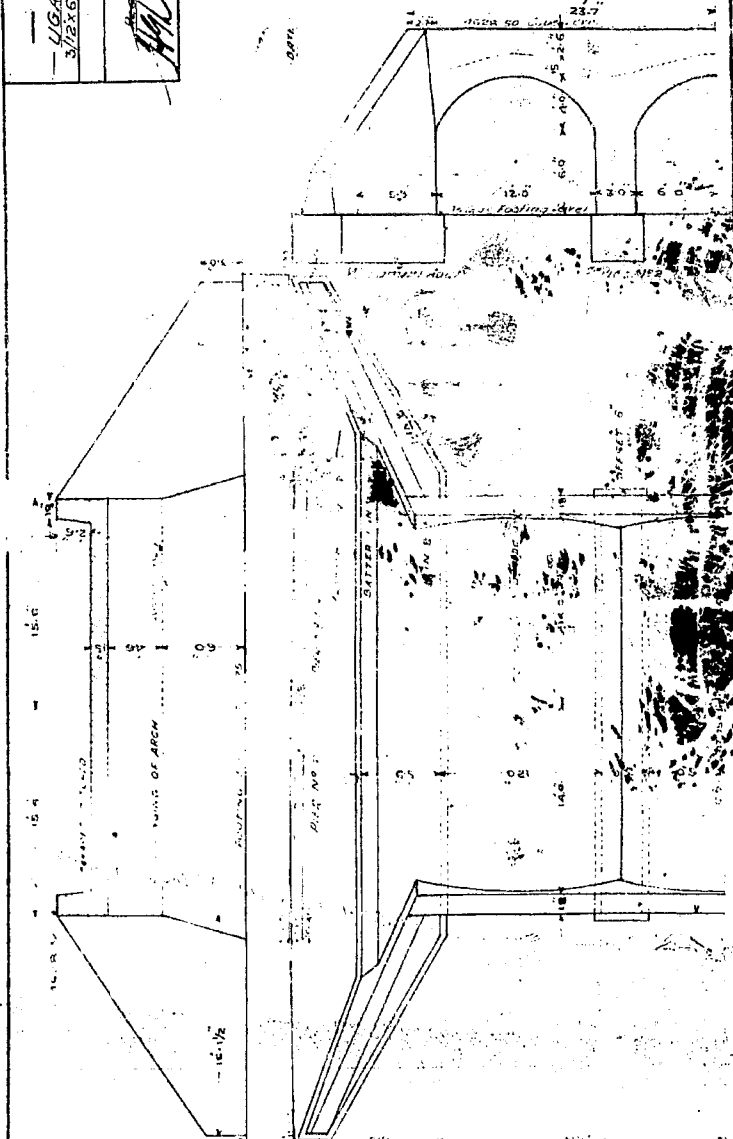
Scale: 1" = 8 Feet

Resident Engineer

H. D. Murphy

THIS DRAWING SHOWS WORK
AS ACTUALLY CONSTRUCTED

MILEAGE
No 38 193/16



K. & U.R.

UGANDA EXTENSION
27 1/2 x 6 ARCH CULVERT

Scale 1" = 8 Feet

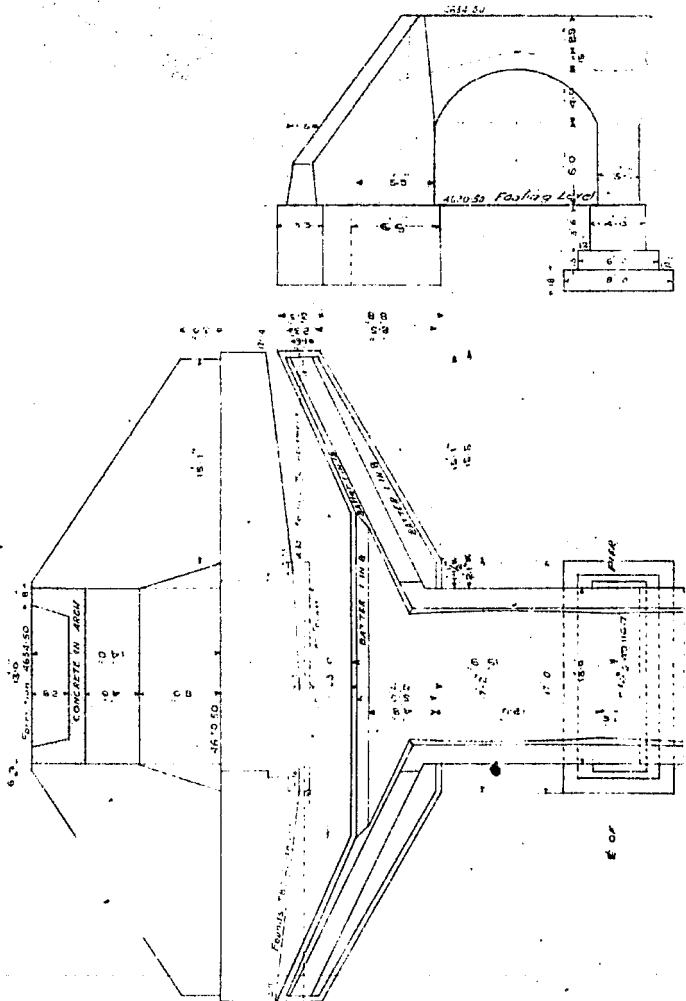
Resident Engineer

A. H. Campbell

THIS DRAWING SHOWS WORK
AS ACTUALLY CONSTRUCTED

DATE

NO 35 MILEAGE
193/21



— K. U. R. —
— UGANDA EXTENSION —
3/6 X 6 ARCH CULVERT
 — Scale 1:1000 —

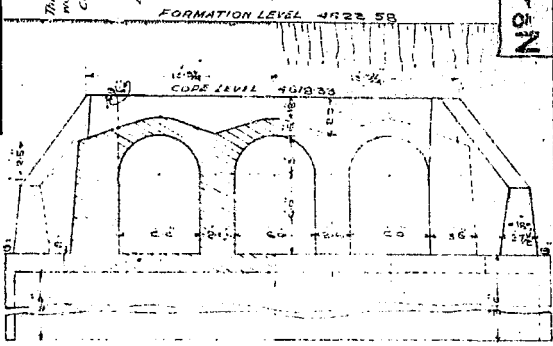
K. U. R.
 Resident Engineer

— Note —
 This Drawing Shows
 work as actually
 constructed

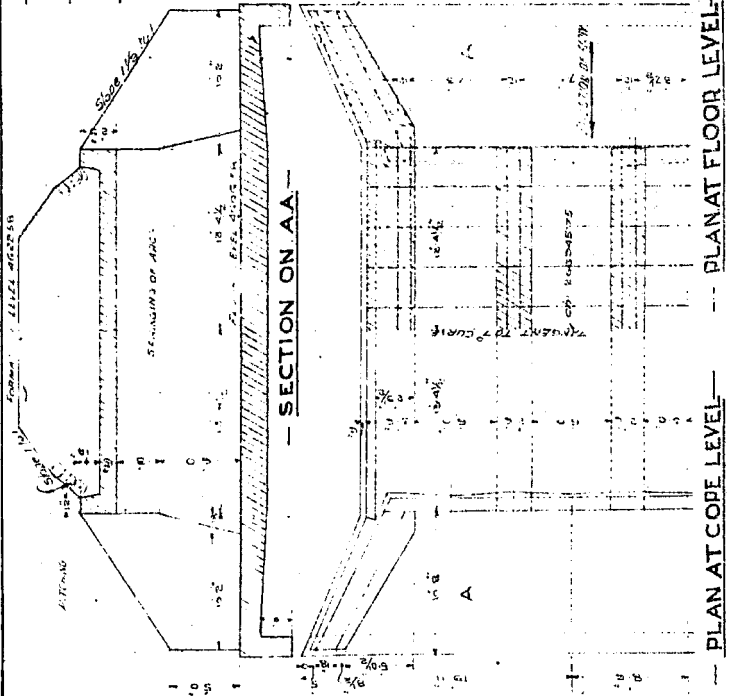
Ref.

№ 40
MILEAGE
197/7

— Note —
 Longitudinal Rails —
 Set 1/4" in each of 4 rails placed
 resting on lower set of transverse rails
 — Lower Transverse Rails —
 Set 1/4" in each of 4 rails placed
 on top of longitudinal rails
 — Top Transverse Rails —
 Set 1/4" in each of 4 rails placed
 on top of longitudinal rails
 for structure 197/7



— HALF SECTION — HALF ELEVATION —



— SECTION ON AA —

— PLAN AT COPE LEVEL —

— PLAN AT FLOOR LEVEL —

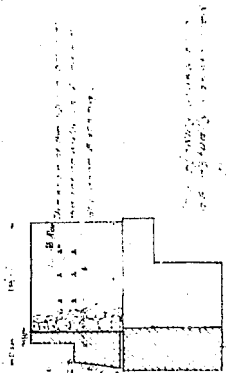
- K. U. R. -

UGANDA EXTENSION
S/GO GIRDER SPAN BRIDGE

- OVER MINGA RIVER -

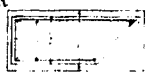
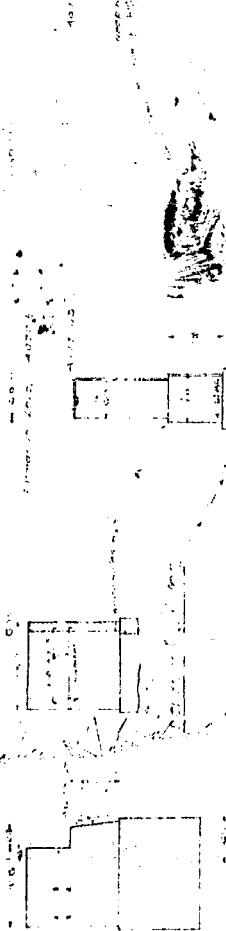
SCALE 1/2" = 1'-0"

J. H. Campbell



DATE

- SECTION IN B -



RIVER

- PLAN -

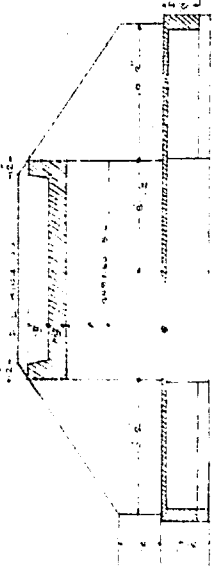
K.U.R.
 UGANDA EXTENSION
 G.I. ARCH CULVERT
 Scale 1/4" = 1'.

A. H. Compton
 RESIDENT ENGINEER

This Drawing Shows work
 as actually constructed

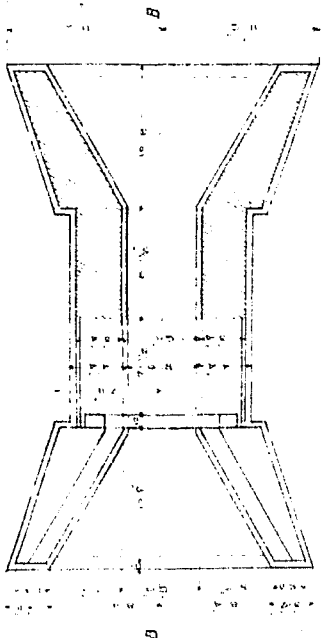
--- Date ---

N^o 42 MILEAGE
 215/4

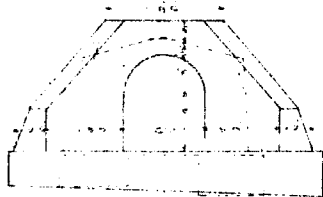


--- Section B.B. ---

foundation on soft bank



--- Plan at Floor Level ---



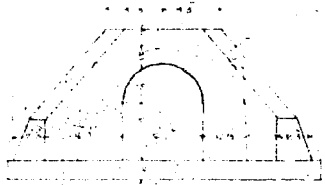
--- Elevation ---

K. U. R.
USANDA EXTENSION
G. A. ARCH CULVERT
100' to 150' feet

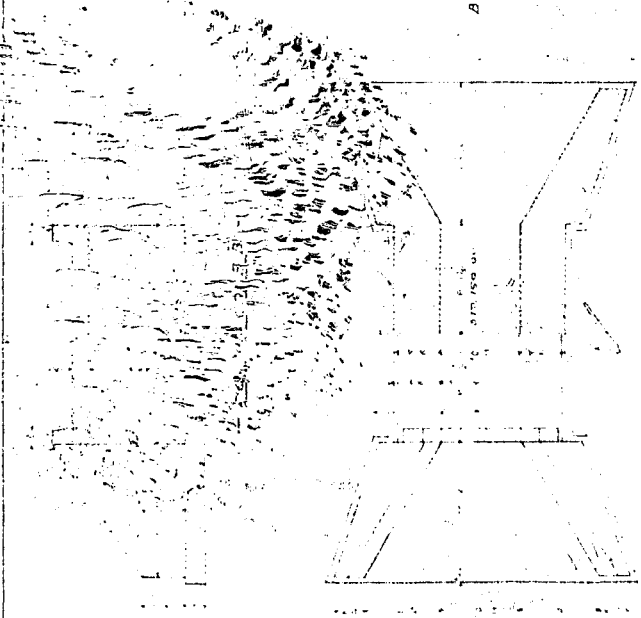
W. C. [unclear]
 Resident Engineer

... original shown with
 ... actually constructed

Note



— Elevation —



— Plan of Floor Level —

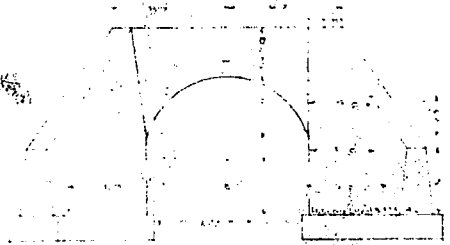
N° 43 MILEAGE
218/15

— K.U.R. —

UGANDA EXTENSION
12' 6" ARCH CULVERT

DESIGNED BY
DRAWN BY

MILEAGE
No 44 222/0



— ELEVATION —

SECTION ON A-B

ELEVATION

B

A

— PLAN —

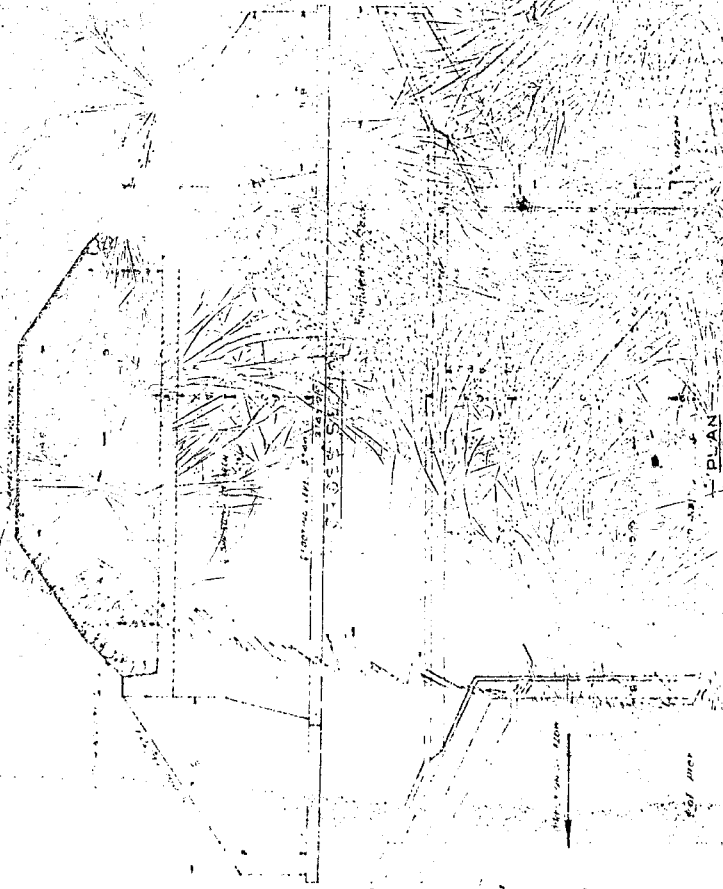
K&NR

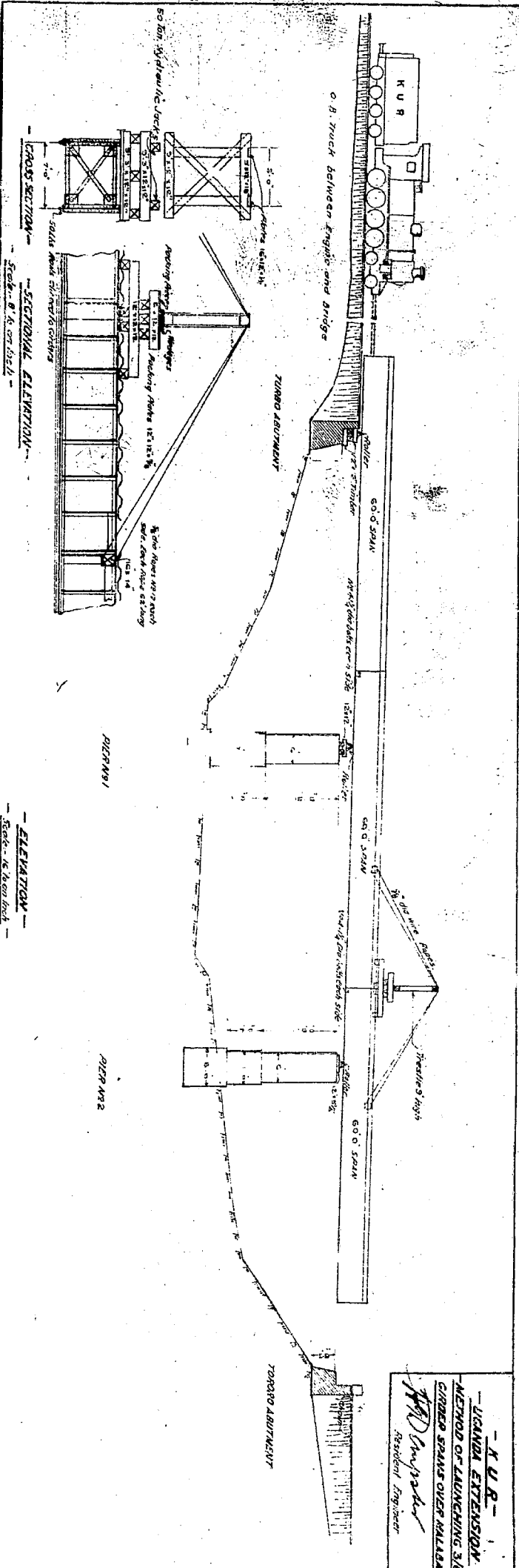
1047 31 E
SWEETEN C. 112

W. H. ...

MILEAGE
223/8

4G





K U R
 - LIGANDA EXTENSION
 - METHOD OF LAUNCHING 3,600
 GIRDER SPANS OVER ALLAGA R.
W. D. Campbell
 Resident Engineer

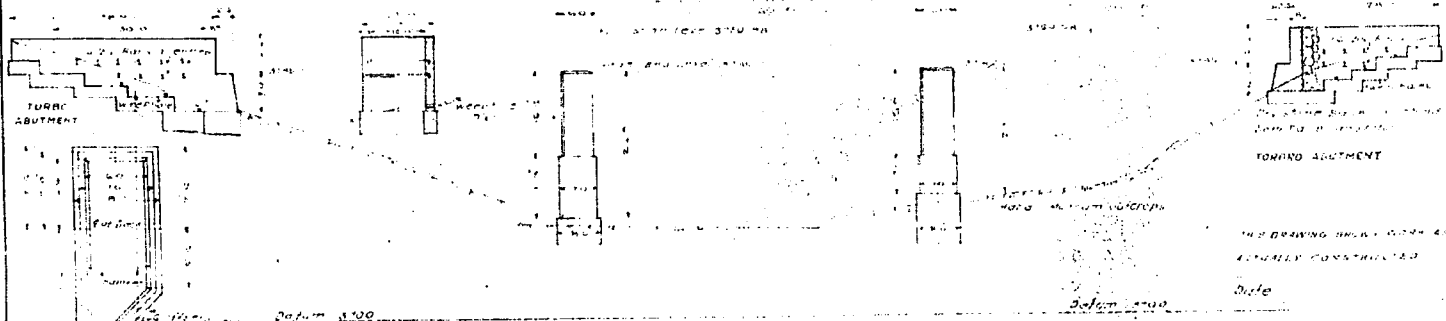
50 ton hydraulic jacks
 CROSS SECTION -
 Scale - 1/4" = 1'-0"

SECTIONAL ELEVATION -
 Scale - 1/4" = 1'-0"

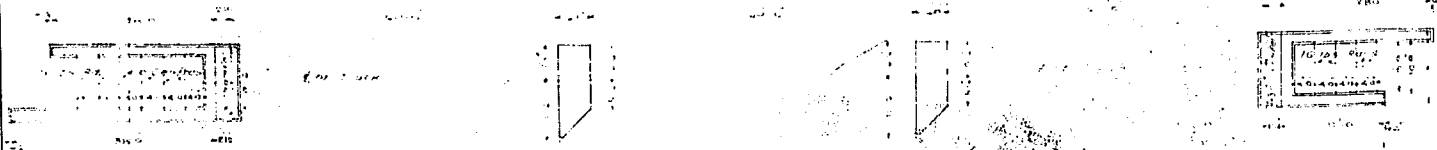
ELEVATION -
 Scale - 1/4" = 1'-0"

K.U.R. — UGANDA EXTENSION. — 3-60' GIRDLER SPAN BRIDGE OVER MALABA RIVER —

— Scale: 1 inch = 40 feet —

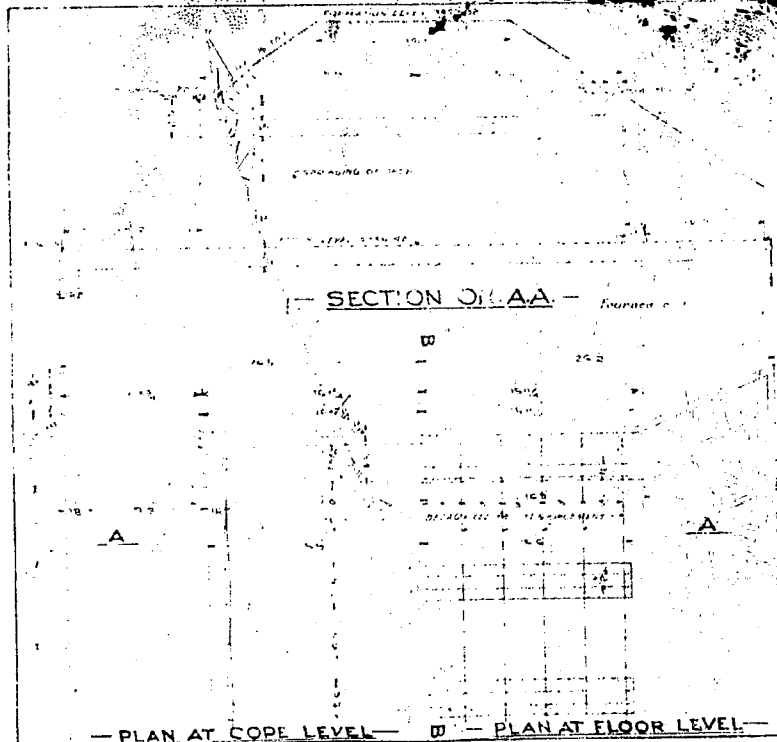


PLAN OF PIER SHOWING
CHAMFER AND OFFSETS



[Signature]
Resident Engineer

N^o 47 MILEAGE
224/18



RAIL REINFORCEMENT

TRANSVERSE SECTION AT EACH END OF THE ARCH CULVERT SHOWING POSITION OF JOINTS AND REINFORCEMENT. AT EACH END OF THE ARCH CULVERT THE TRANSVERSE SECTION SHALL BE AS SHOWN. JOINTS SHALL BE LOCATED AT THE CENTER OF THE ARCH CULVERT. JOINTS SHALL BE REINFORCED WITH 2 NO. 4 BARS.

K. U. R.
UGANDA EXTENSION
3-6.5' ARCH CULVERT
 SERIAL NUMBER
H. H. Omputa

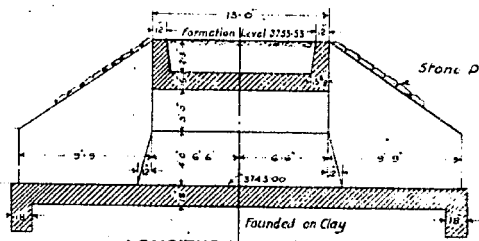
No 48	MILEAGE 227/3
-------	---------------

K. U. R.
UGANDA EXTENSION
2-6' x 4' ARCH CULVERT
 - Scale - 8" to 1' -

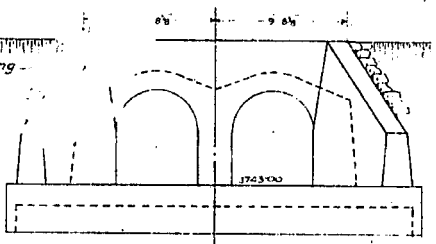
H. D. [Signature]
 Resident Engineer

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

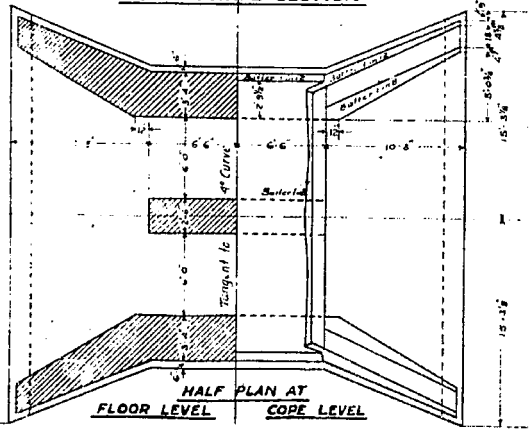
DATE _____



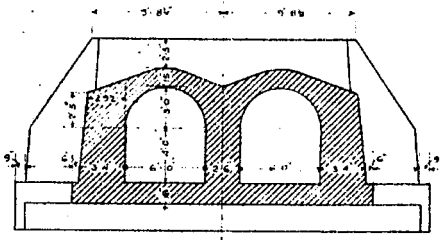
LONGITUDINAL SECTION



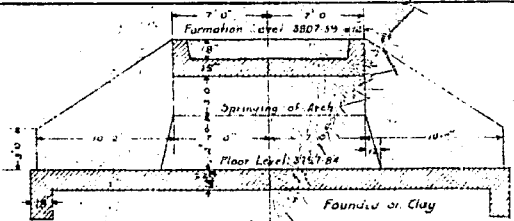
ELEVATION



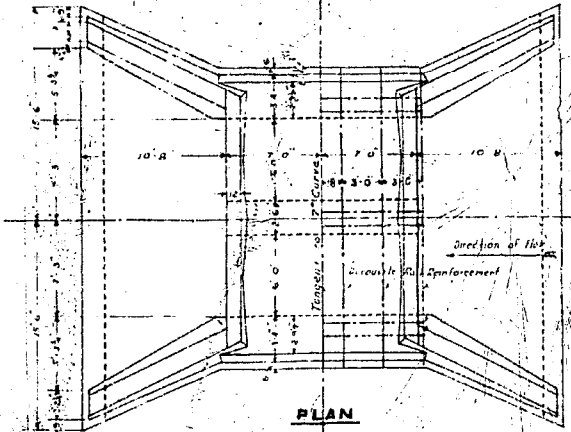
HALF PLAN AT FLOOR LEVEL AND COPE LEVEL



CROSS SECTION

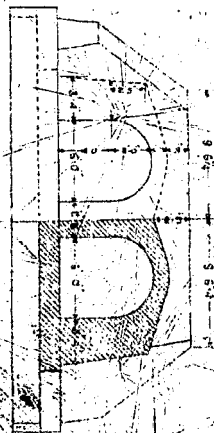


SECTION A.A.



PLAN

RAIL REINFORCEMENT
 Longitudinal Rails - 3/4" dia. long train rails
 - 200 lbs. weight of lower Transverse Rails.
 Lower Transverse Rails - One set of above
 of 1/2" dia. concrete and placed at 3'0" centres.
 made up of 1/2" and 1/4" rails, finished joints.
 1/2" Transverse Rails - 1/2" dia. below
 surface of 1/2" dia. placed at 3'0" centres.
 Each Rail at least 15' long.



HALF ELEVATION

HALF SECTION

- K. U. R. -
- UGANDA EXTENSION -
2-6'0" x 4'0" ARCH CULVERT OVER
MANUMITY RIVER
 - Scale: 1" = 8 Feet -

A. W. Dempster
RESIDENT ENGINEER

**THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED**

DATE _____

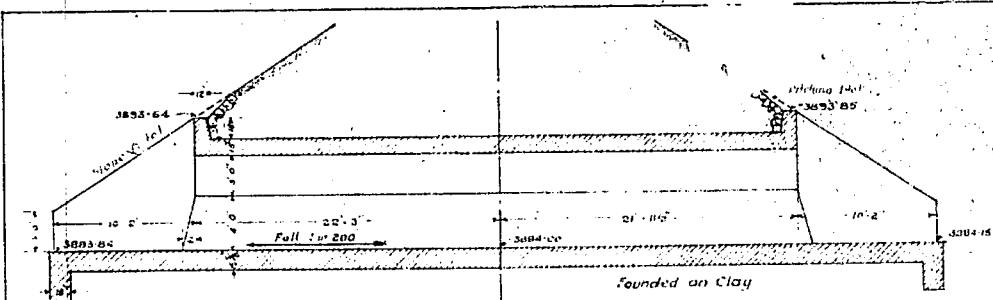
**No 50 MILEAGE
 230/1**

— K. U. R. —
 — UGANDA EXTENSION —
 2-6 D x 4-0 ARCH CULVERT OVER
 RADIKA RIVER
 — Scale = 1" = 8 Feet —

H. D. Crisp
 RESIDENT ENGINEER

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

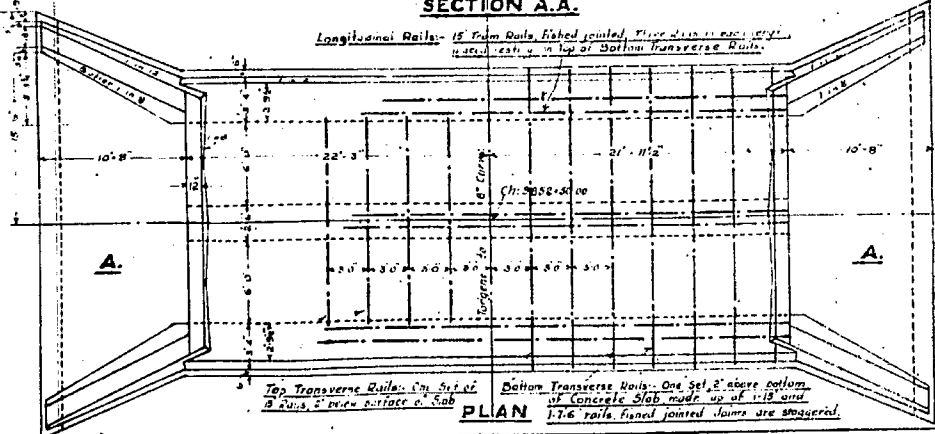
DATE



SECTION A.A.

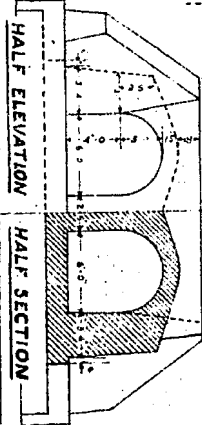
founded on Clay

Longitudinal Rails: 15' Tram Rails, fisher jointed. Five rails in each cry. (actual resting in top of Bottom Transverse Rails.



Top Transverse Rails: One Set of 3 Rails, 2' above surface of Slab
Bottom Transverse Rails: One Set, 2' above bottom of Concrete Slab made up of 1-13 and 1-7.6 rails, fisher jointed. Joints are staggered.

PLAN



HALF ELEVATION

HALF SECTION

No 51 **MILEAGE**
231/19

K.U.R.

UGANDA EXTENSION

2 1/2 x 4 ARCH CULVERT

Scale 1" = 4 Feet

Resident Engineer

H. D. Dempster

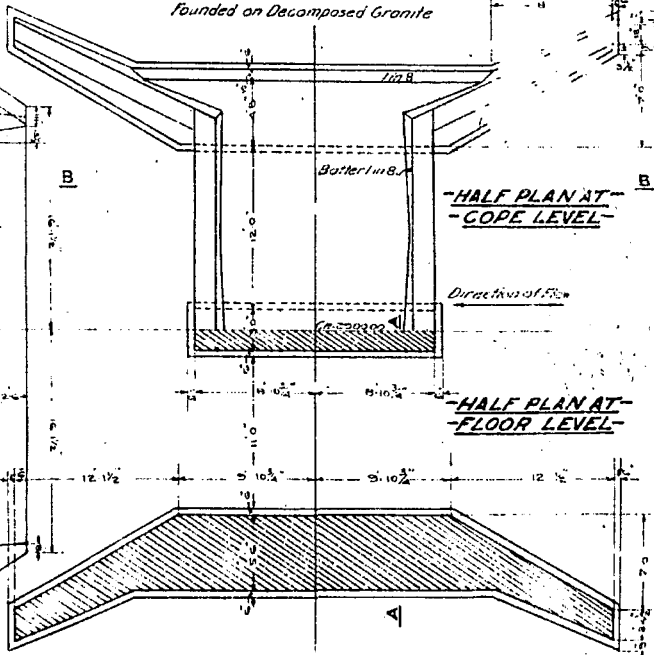
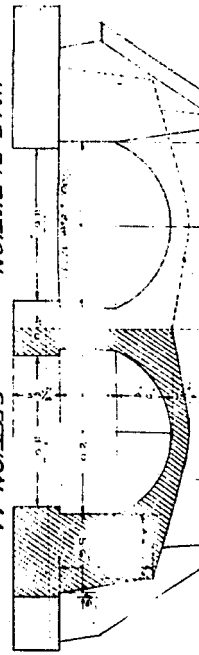
*This drawing shows work as
actually constructed*

Date

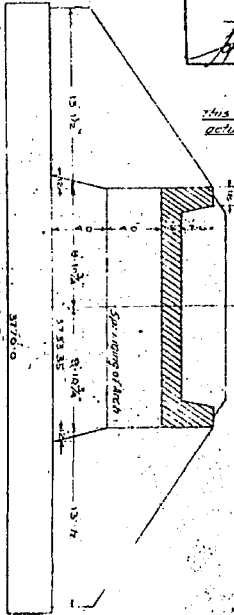
Founded on Decomposed Granite

HALF ELEVATION

SECTION A.A.



SECTION B.B.



No 52 MILEAGE
235/6

LONGITUDINAL RAILS -

LOWER TRANSVERSE RAILS -

TOP TRANSVERSE RAILS -

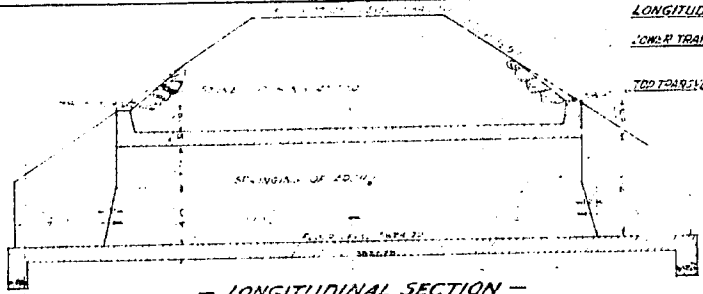
1. Slings, and 2. 1/2" to 3/4" from No. 8
3. Slings, and 4. 1/2" to 3/4" from No. 8
5. Slings, and 6. 1/2" to 3/4" from No. 8
7. Slings, and 8. 1/2" to 3/4" from No. 8
9. Slings, and 10. 1/2" to 3/4" from No. 8
11. Slings, and 12. 1/2" to 3/4" from No. 8
13. Slings, and 14. 1/2" to 3/4" from No. 8
15. Slings, and 16. 1/2" to 3/4" from No. 8
17. Slings, and 18. 1/2" to 3/4" from No. 8
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91. Slings, and 92. 1/2" to 3/4" from No. 8
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95. Slings, and 96. 1/2" to 3/4" from No. 8
97. Slings, and 98. 1/2" to 3/4" from No. 8
99. Slings, and 100. 1/2" to 3/4" from No. 8

- K.U.R. -
UGANDA EXTENSION
2/6x4 ARCH CULVERT

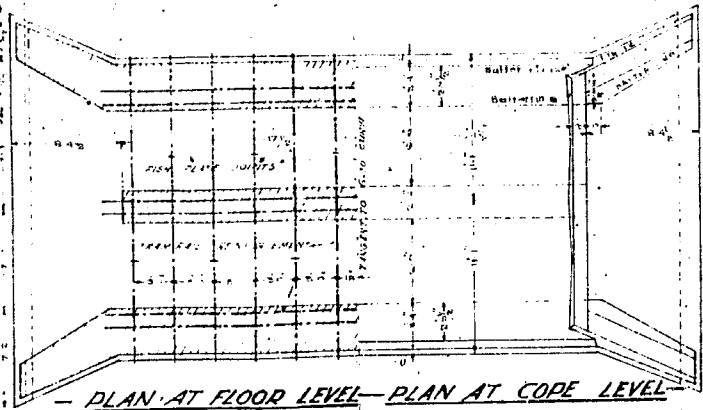
- Scale 8/16 inch -
A.H. Clarke
Resident Engineer

THIS DRAWING SHOWS WORK
AS ACTUALLY CONSTRUCTED

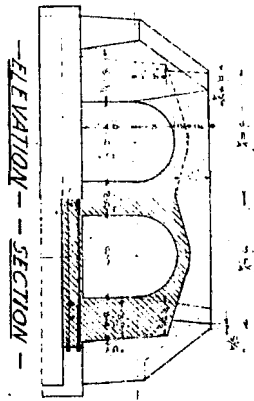
DATE _____



- LONGITUDINAL SECTION -

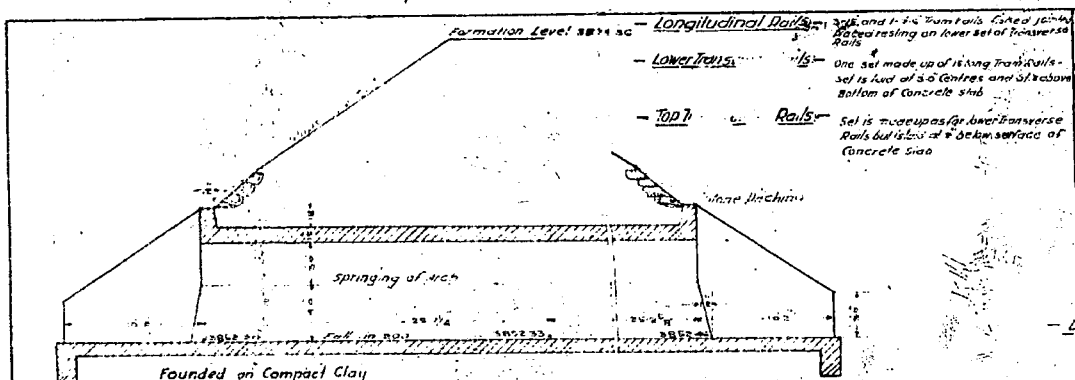


- PLAN AT FLOOR LEVEL - PLAN AT COPE LEVEL -



- ELEVATION SECTION -

N^o 54 **MILEAGE**
238/11

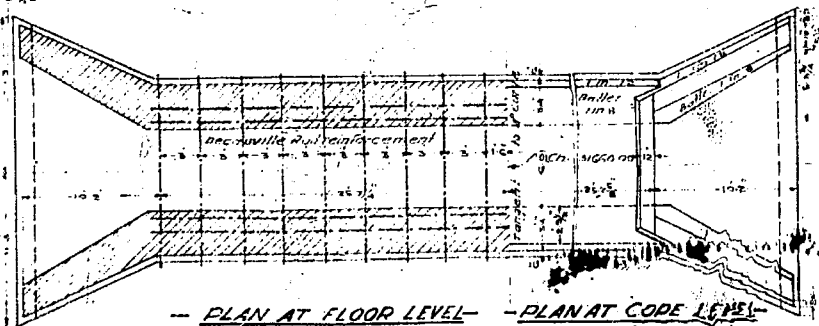


- Longitudinal Rails - 3/8" and 1/2" Trans rails lined joining
 Base resting on lower set of Transverse
 Rails
 - Lower Trans. Rails - 1/2" One set made up of 1/2" long Trans rails -
 set is laid at 3-6 Centres and 5/8" above
 Bottom of Concrete slab
 - Top Rails - One set is made up for lower transverse
 Rails but is 1/2" at 9" below surface of
 Concrete slab

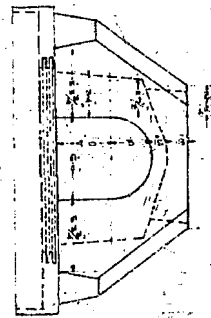
- K.U.R. -
 - UGANDA EXTENSION -
 - 1-6x4 ARCH CULVERT -
 - Scale: 3/16 inch -
A.D. Campbell
 Resident Engineer.

THIS DRAWING SHOWS WORK
 AS ACTUALLY CONSTRUCTED

- LONGITUDINAL SECTION -



- PLAN AT FLOOR LEVEL - PLAN AT CODE LEVEL -



- ELEVATION -

No 55 MILEAGE
 240/15

1928

Kenya
Legation

No. 15297

SUBJECT

C O 533 / 379

Estates of Mrs. C. M. near Jackson
and heirs of W. T. Bennett

Previous

Subsequent

61517/29
6631 33

1928

*Kenya
Uganda*

No. 15297

SUBJECT

C O 533 / 379

*Estates of Am. C. near Jackson
and heir of G. W. Z. Bennett*

Previous

Subsequent

see 61517/29

6631 35

1. known Agents. _____ 11th July 28
(Asks whether any local legislation has been
passed in Kenya or Uganda appropriating
the hereditary revenues of the crown.)

2

^{Shirway}
Mr. ~~Shirway~~

Can you please say whether there
is any such legislation in Kenya
& Uganda?

ML

18/7/28

Mr. Bushie

Mr. Shirway has drawn my attention
to Section 11 (3) of the Kenya

Public Trusts Ordinance 1915 and to

Section 24 (2) of the Uganda

Administration General Ordinance 1918.

Can these sections be taken as

constituting the "local legislation"

referred to in this enquiry?

Yours

20/7/28

Yes! The latter have been administered
locally since 1917

comes
with

no. 122
Date: 23/7/28

D. Duncan Jackson died actually
in Uganda, but / his case was no
doubt dealt with at K. O. R. headquarter
at Nairobi. It is unclear as a matter
of fact whether his estate is being dealt
with by the Admin. General of Uganda
or the Public Trustee of Kenya & the C.A.
apparently were only in communication
with the General Manager & admin. Gen.
But the minute is drafted way so

8 Dec
23/7/28

Note

no. 122/28

file

2

to proceed

to (1) (the minute) 23/7/28

1. The estate of the late Mr. D. Duncan Jackson, who died in Uganda in August 1927. Suggests that the estate should be passed to the Public Trustee of Kenya & the C.A. as the Public Trustee of Kenya & the C.A. is the administrator of the estate. It is suggested that the Public Trustee of Kenya & the C.A. should be asked to issue the necessary orders for the issue of the estate to the Public Trustee of Kenya & the C.A. and to the Public Trustee of Kenya & the C.A. as the Public Trustee of Kenya & the C.A. is the administrator of the estate.

we shd presumably send a
letter to Uganda saying that the
Treasury is connected with the
admin. of the estate of the

late Mr. Duncan Jackson of the K. O. R., the
Treas. have raised the question whether
to be dealt with at A or B. I suggest
to be dealt with at B.

In the Barbados case the
Treasury made a similar suggestion
which was put to the Gov. with
a request to see the report in J.P.
But nothing further has happened
apparently. It seems
reasonable why we shd ask to
see the report in J.P.

I shd not have thought that
it was unnecessary for the Co to
be dragged into the question in paras
6 & 7. But if anybody is to
do the advertising surely it shd be
the C.A.? We might reply to
the Treasury sending a copy of
the proposed report to Uganda; &
saying that a request para 6 & 7
that the S.O. does not consider that
he is concerned & suggest that they
shd communicate direct with the C.A.
& copy copies to C.A. 17/7/28
G. Easton

returning the
in orig

I should send a copy of the letter from the Treasury to the Crown Agents; say we concur, and suggest that they should get into touch with the Treasury Solicitor accordingly. And then you might send a copy of the correspondence to the territories concerned, inviting them to consider the question of legislation.

9.1.29.

- (1) as regards the suggestion para 5 of the letter, after some delay, a letter for the Commission on the subject of the proposed legislation will be sent to the territories.
- (2) as regards para 6 of the letter, we can set out our views in a separate letter.

Df (a) is in view and I am suggesting that Syg will consider the suggestion in para 5 & that he can send a suggestion in para 6 to the territories. I have been in touch with the territories in this matter.

(b) to P.A. of 1.2.29 - enclosing copy of 3 (without encl) & reply to report that CA will act in accordance with the

suggestion in para 5 & communicate direct with Treasury Solicitor.

A. Main note. W.G. & J. P. Smith
 cons -
 Pass up through S. D. of the Treasury

Act
 W. 1. 29
 1. 29

to the Treasury (No 3 and will retained)
 L.A. (in respect of 2)

~~DESTROYED UNDER STATUTE~~

~~DESTROYED UNDER STATUTE~~ 26 April 1929
 The Treasury Solicitor has reported that the Commission has now come forward and produced a report of Administration of the deceased's estate. The CA has already been requested to obtain the balance of the deceased's estate and send it over to the Treasury Solicitor, presumably the latter will pay it to the executor. Ask (6) H.P. and send copy of 163 to S.A. for info. H.P. 163

Returned to (6) today

Ch. ? action as above has been taken.

The CA know no need to act. Not can be put by

Gen. H.P. 163

See A above. Action has been taken. 163/164 General

W. 1. 29
 1. 29

SW 4

Mr. Eastwood 11/1

Mr. Seel 14/1.

Mr. Smith 14/1/29

Mr. Bushe 15/1

Mr. Robinson 16/1

Sir E. Harding

Sir J. Shuckburgh

Sir G. Grindle

Sir C. Davis

Sir S. Wilson

Mr. Ormsby-Gore

Lord Lovat

Mr. Amery

Downing Street

19 January, 1929.

Handwritten signature/initials

Amery

Copy to L.C. 10 1929

Sir,

I am etc. to acknowledge the receipt of your letter No.S.33573, of the 24th of December 1928 on the subject of the disposal of the balance of the estate of Mr. Duncan Jackson, and to request you to inform the L.C. of the Treasury that he ~~was~~ ^{will consider} the

suggestion in para 5 of your letter ^{in reference to which} that legislation should be passed ^{the question of} in Uganda ^{specifically} with the disposal of bona vacantia

DRAFT

SECRETARY,

SURVY.

if not dealt with in existing legislation, the matter should be made the subject of further specific legislation.

encll. in orig

Revs. to G.D

3. Mr. Amery concurs in the ^{above} suggestion made in para 5 and 7 as to the administration of Mr.

Jackson's estate, and the Crown ^{in the Dominion} requested to Agents have been instructed ^{to proceed} accordingly. They will communicate ^{directly with the Secretary} ^{to the matter}

Telephone No.: VICTORIA 1234.

6 3

Any reply to this letter should be addressed to—

THE SECRETARY,
TREASURY,
WHITEHALL LONDON, S.W.1,
and the following number quoted.



TREASURY CHAMBERS.

December 1928.

Sir,

I am directed by the Lords Commissioners of His Majesty's Treasury to transmit herewith a copy of a letter from the Crown Agents to the Colonial Office dated the 17th June last with enclosures, in original, for return, on the subject of the disposal of the balance of the estate of Ducau Jackson who died in Uganda in August 1927, and to request you to acquaint the Secretary of State as follows.

1. My Lords understand that your department has already been consulted by the Crown Agents with reference to the fact that the Uganda Protectorate that in the year 1900 when it was first proclaimed there exists legislation which has the effect of appropriating the hereditary revenues of the Crown, such as bona vacantia, to the public revenue of the Protectorate, and further that in general bona vacantia fall to the Crown in that part of the Empire where they are situate.

Copy to the Secy to the Secy

The Under Secretary of State,
Colonial Office.

3.

reference to other matters, and, in the absence of a specific reference to that peculiar kind of Crown property, should be held to cover bona vacantia.

A

A

B

My Lords suggest that Her Majesty's consideration therein that to resolve doubt as to the nature of the property vacantia the appropriate legislation should be after dealing specifically with the disposal of bona vacantia. In this case I wish to refer to Treasury letter of the 11th of January 1910 in answer to Sir Robert Finlay's letter of the 11th of April 1909 (1005/10) respecting the treatment of bona vacantia as respects the Crown in the Colony of Barbados.

Tr 45693
Dated 11/11

As to the case of Jackson the Lords observe that there is evidence that the deceased was lawfully married and that until proper inquiries had been taken to ascertain whether the widow has survived her husband, it cannot be assumed that there is a presumption that he is deceased or that his estate has passed to the Crown, whether by way of appropriation to the public service or to Uganda revenues. My Lords consider, therefore that the usual inquiries should be made and advertisements issued for the widow and next of kin.

reference to other matters, and, in the absence of specific reference to that particular kind of Crown property, it must be held to refer generally.

A
A
B

The Lords observed that the consideration therein that to resolve doubts as to the true meaning of the question the appropriate construction should be used in giving effect to the disposal of the vacant. In this case it is to refer to the words of the deed which refer to the answer to Sir Robert Bannister's letter of the 17th July 1666 (copy) respecting the transfer of the vacant lands in the Duchy of Lancaster.

Ex 48693
1911
Duchies

According to the case of *London* the Lords observed that there is evidence that the Crown was lawfully made and that this paper of the Crown taken to ascertain whether the land was lawfully made, it cannot be argued that there is any evidence to be deduced that the state was made to the Crown, another form of appropriation to the Crown, or to the revenues. The Lords consider, therefore that the usual inquiries should be made in advertisements issued by the Crown and next-

8 X 15297/100
Kinn
100

- Mr. Lee 23/7/28
- Mr. Lee 25.9
- Mr. Bunker 24
- Mr. Bottomley
- Mr. E. J. Harding
- Sir J. Shuckburgh
- Sir G. Grindle
- Sir O. Davis
- Sir S. Wilson
- Mr. Ormsby-Jones
- Lord Local
- Mr. Amery

25 JUL
10 17

P. 11

25.9

Crown Agents 23 Jul. 28

For under
U. in a bill

Your minute P/Estates

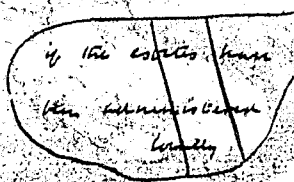
DRAFT.

minutes for Mr.
Lee's views

2723 of the 11th July
regarding the disposal of the
the estate of
the late Mr. D. Jackson
and of the late Miss
C. W. T. Garrett

We are advised

Sections 11 (3) of



the Kemp Public Trustee's
Ordinance 1925 and 24 (2)
of the Highways Act 1925

General's Ordinance 1918

... the ...

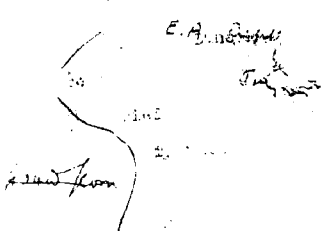
... representing the

... of the Commission

bona vacua

... - referred

... the ...
of the states ...
... that any ... will



We are advised however that in any case bona

vacua calls to the Commission in that case for the ...

... ~~...~~ ...

... of ...

General's Ordinance 1412

Establish the local

legislation appropriate to the

needs they receive of the Commission

Bona fide

... - required

of the states in which they

that any assistance

E.A. ...

from

We are advised however that in any case bona

fide calls to the Commission in that case for the

... ~~Situation~~

... of Lee

P/Estates 2723.

9
WD

12 JUL 1928

Mr. O.O.R. Williams.

COLONIAL OFFICE.

File 30967EA.
File 17600EA.

We have at present on hand two estates, one of a Kenya Uganda Railway official, the late Mr. Duncan Jackson, Plate-laying Assistant, Construction, and the other of a Uganda official, Lieut. C.W.T. Garrett, Administrative Officer. In both these cases it has transpired that there are no relatives who are entitled to the estates and we have accordingly notified the Treasury Solicitor of the fact

As a matter of fact, in the case of Lieut. Garrett we were approached on the subject, but in the case of Mr. Duncan Jackson we were not quite clear as to our action and therefore asked the Treasury what line should be taken.

We have now been informed by the Treasury Solicitor that they will take over the Colonial portions of both estates unless there is any "local legislation appropriating the hereditary revenues of the Crown, e.g. bona vacantis", in which case, of course, the balance due and the effects would revert to the Colonial Government.

Will you please let us know whether any such local legislation has been passed in the Colonies in question.

H.D. ...

"P" Department,
Crown Agents,
11/7/28.

mh